

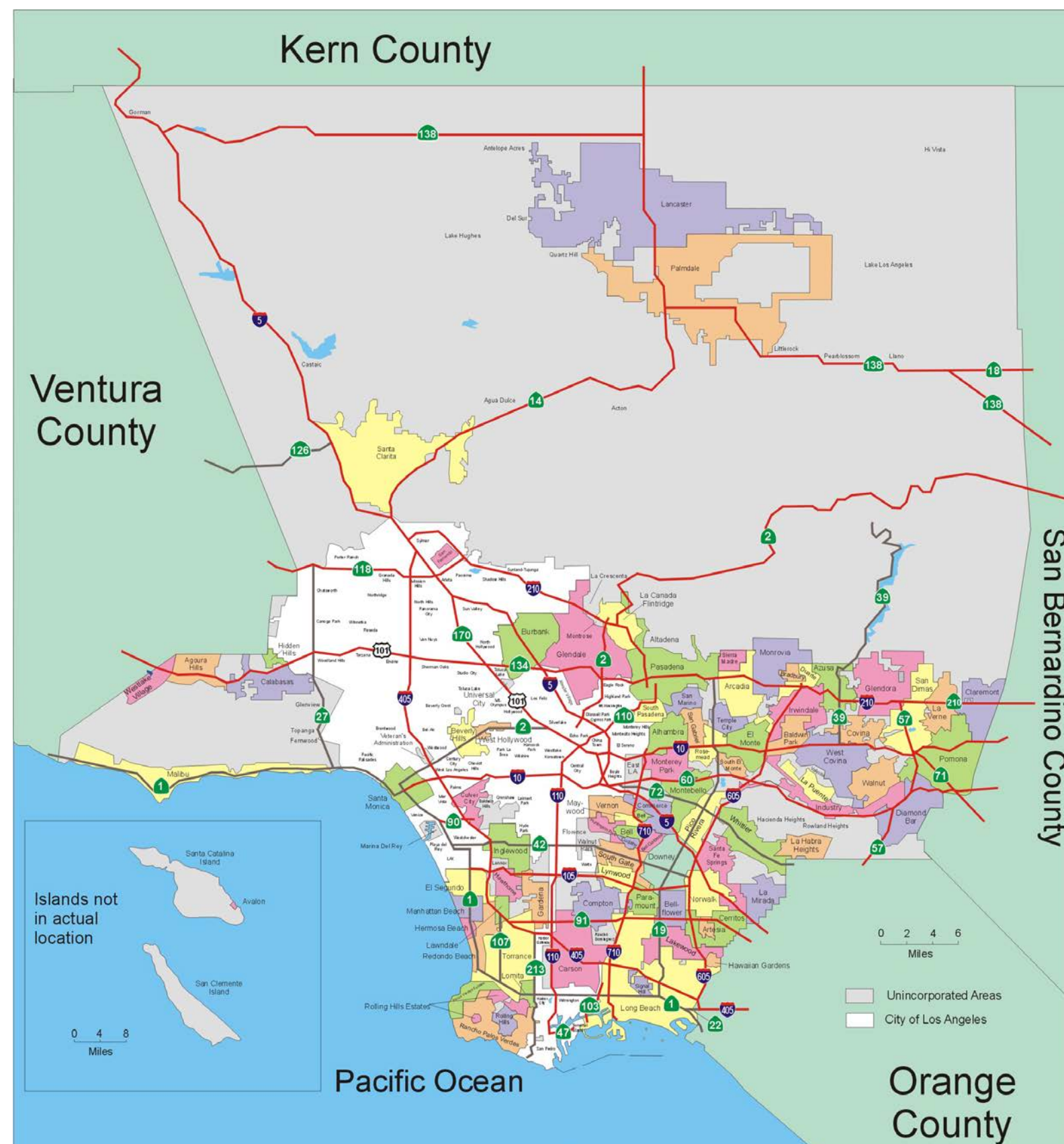


# **Foothill Gold Line**

**May 19, 2021**



# About the Foothill Gold Line



- Los Angeles County's population will grow by 16% to 11.5 million by 2040.
- During that same period, **the San Gabriel Valley will grow by 11.4% to more than two million.**
- The Foothill Gold Line cities will take on the majority of SGV's expected growth.



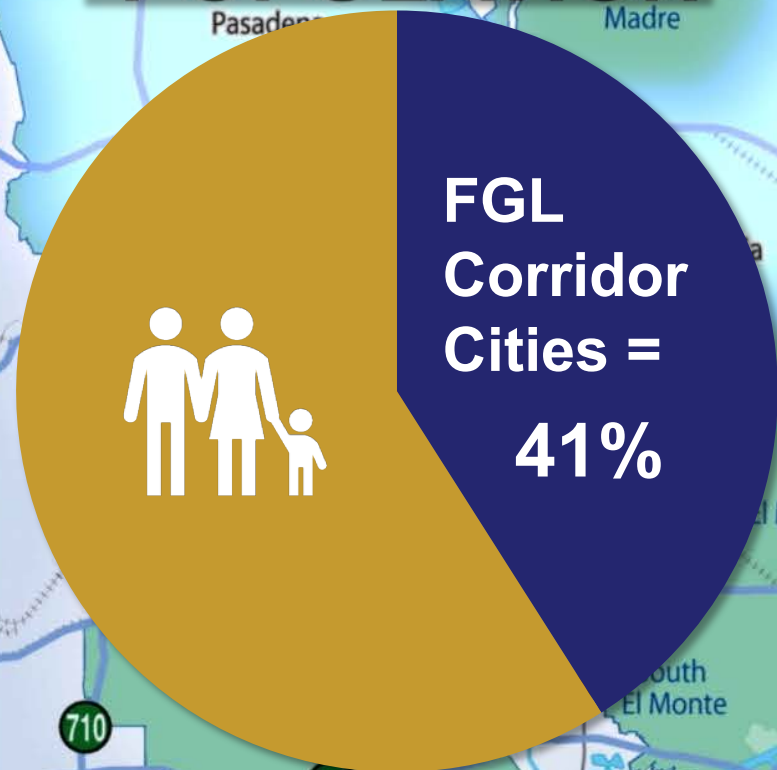
Foothill Gold Line



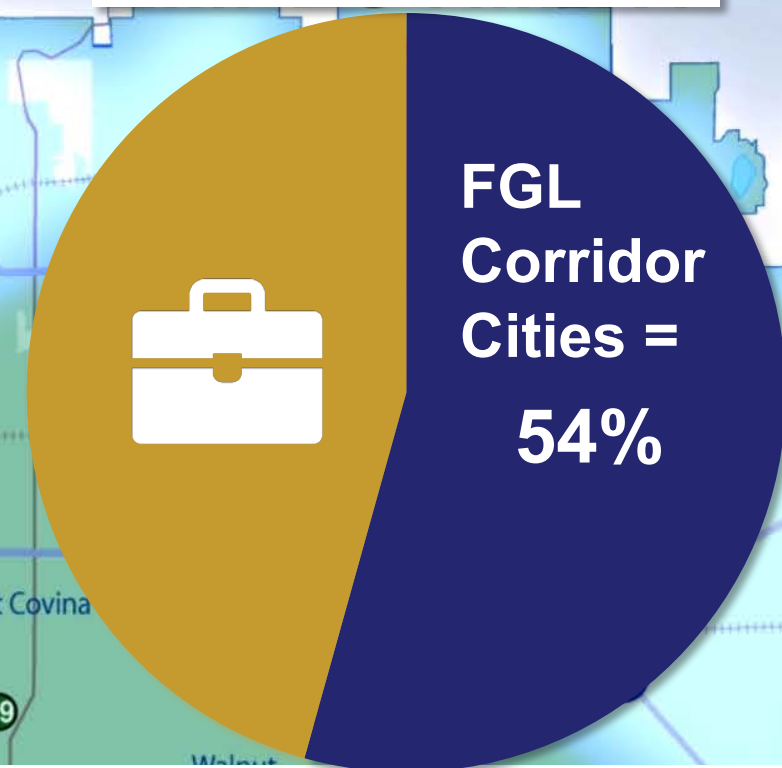
# San Gabriel Valley's Growth 2012-2040

EXPECTED GROWTH: 2012 - 2040

## POPULATION



## EMPLOYMENT



- Foothill Gold Line cities are growing faster than the SGV as a whole and taking on the majority of growth in the subregion:
  - 41% of all SGV population growth
  - 54% of all SGV job growth



Foothill Gold Line



# Nearly Three Million Trips Each Day



Of the 2.8 million trips taking place each day within and around the Foothill Gold Line corridor, **nearly all are by car** - only 3% are by transit.



Foothill Gold Line



# Construction Authority

- Construction Authority was created by state legislation in 1998 to plan, design and build the Metro Gold Line from Union Station east to Montclair
- Enabling legislation provides all necessary powers to complete the project
- Construction Authority is overseen by board of directors
- Board of directors receives feedback from all corridor cities through JPA and TAC
- Construction Authority works closely with Metro -
  - Roles and responsibilities formalized in Master Cooperative Agreement
  - Construction Authority builds the project per Metro's design and safety specifications
  - Both agencies play a role in project safety, planning and education



# Construction Authority Board of Directors



**Melchor Iloin**  
City of Los Angeles Alt.



**Ed Reece**  
SGVCOG Rep.



**Chair - Tim Sandoval**  
LA Metro Rep.



**Vice Chair -Robin Carder**  
City of Pasadena Rep.



**Mendell Thompson**  
City of So. Pasadena Rep.



**Dan Evans**  
City of So. Pasadena Rep.  
(Non-Voting)



**Alan Wapner**  
SBCTA Rep.  
(Non-Voting)



**Gene Masuda**  
City of Pasadena Rep.  
(Non-Voting)



**Tony Tavares, PE, PMP**  
Governor's Rep.  
(Non-Voting)



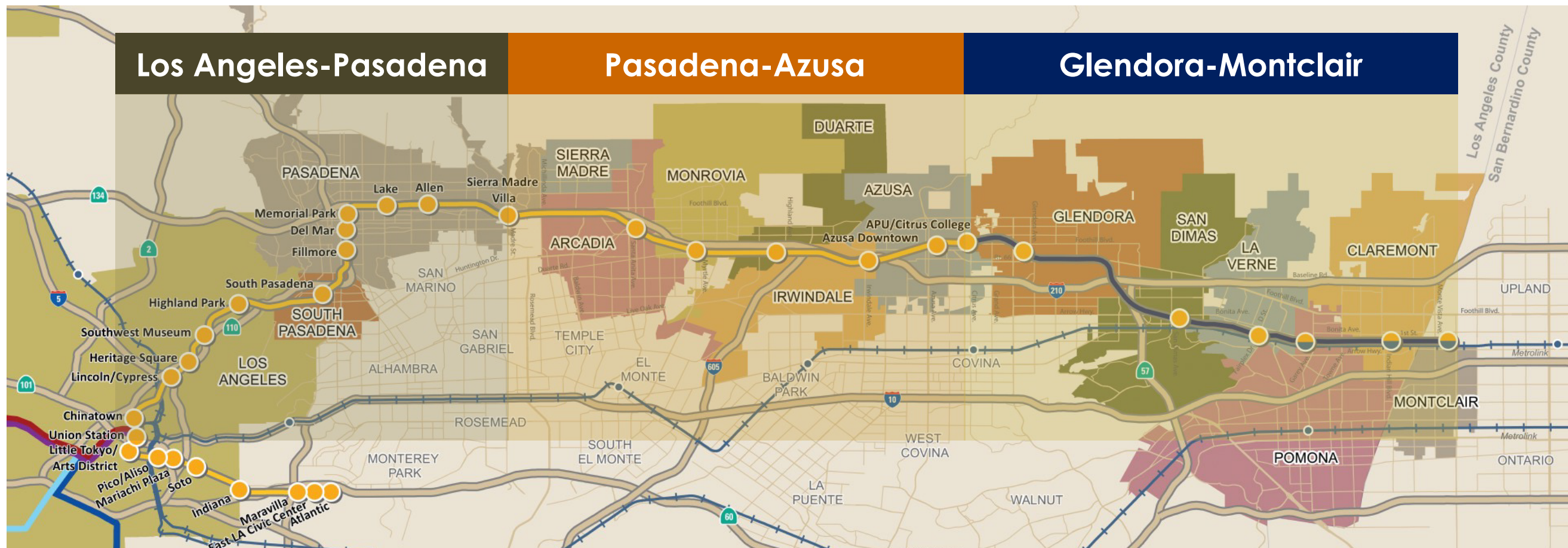


# Grassroots Approach



- The Construction Authority works closely with the corridor cities throughout planning, design and construction phases
- The agency's governance structure includes representation by all corridor cities:
  - Elected Officials – Joint Powers Authority
  - City Staff – Technical Advisory Committee

# Gold Line's 3 Phases = \$3.5 Billion Investment



- Los Angeles to Pasadena – Completed On Time/Under Budget (2003)
  - 13.7 Miles, 3 Cities, 13 Stations
- Pasadena to Azusa – Completed On Time/Under Budget (2015)
  - 11.5 Miles, 5 Cities, 6 Stations
- Glendora to Montclair – Broke Ground December 2, 2017
  - 12.3 Miles, 6 Cities, 6 Stations (possibly to be built in two phases)



Foothill Gold Line



# Part of LA County's Growing Rail Network



County Rail and Busway Network with Measure R and Measure M

Source: LA Metro





# Glendora to Montclair Project Segments



Secured funding for the Glendora to Pomona project segment is mostly coming from Los Angeles County's Measure M half-cent sales tax (2016). Additional funding is coming from residual Measure R (2008) funds not needed to complete the Pasadena to Azusa segment and a State of California Greenhouse Gas Reduction grant (part of SB1).





# Broke Ground - December 2017



# First Two Contracts Complete



In 2018, W.A. Rasic Construction Co. completed a dozen utility relocation and utility protection projects along the project corridor.

In 2019, Mass Electric Co. completed the pole elimination project, which made room for the light rail system by moving underground communication lines for the freight system that will share the rail corridor.



Foothill Gold Line



# Design-Build Contract Signed with Kiewit-Parsons, Joint Venture (KPJV)



Foothill Gold Line



# Glendora to Pomona Project Overview

- 9.1 miles of light rail track (mostly at grade)
- Track is generally within existing, owned right of way
- Overhead electric wires above tracks power trains
- Rail corridor is 100% shared with BNSF (tracks are NOT shared)
- Four new stations (Glendora, San Dimas, La Verne, and Pomona)
- New Pomona Station will provide pedestrian access to/from the Pomona North Metrolink Station
- 21 at-grade (street level) crossings
- 19 New/Renovated Bridges
- 9 Traction Power Supply Substations within or adjacent to the right of way, located 1.0 to 1.5 miles apart
- Pedestrian access from both sides of station platforms at all stations
- Four intermodal parking facilities with bike parking, EV spaces, drop-off and shuttle/bus zones

*Contract Option allows design-build team to build additional project scope from Pomona to Montclair if funding is secured by October 2021.*



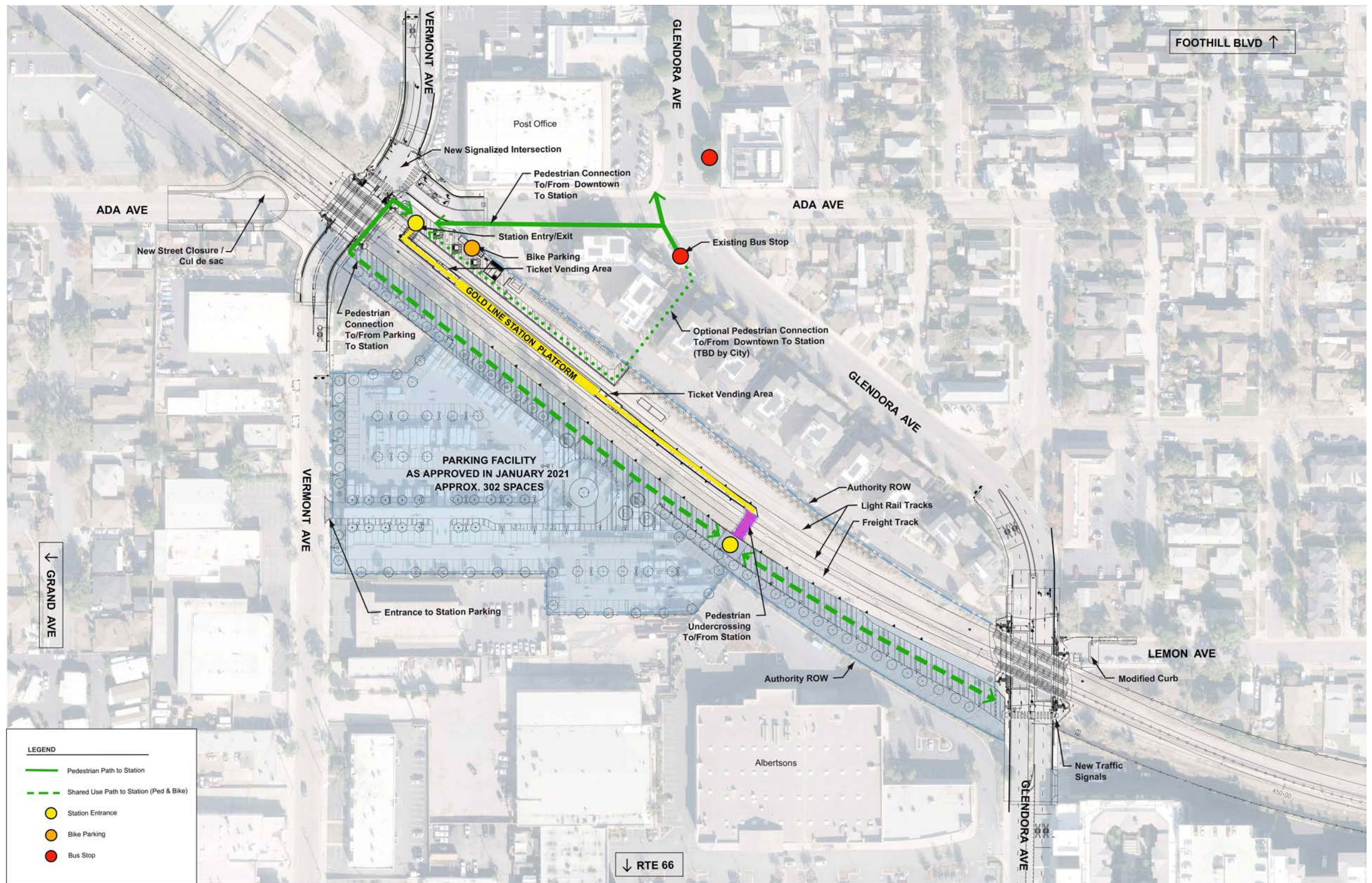
**Foothill Gold Line**



# **Glendora to Pomona Stations**



# Glendora Station Site Plan



As approved in January 2021, the parking facility will have approximately 302 spaces.

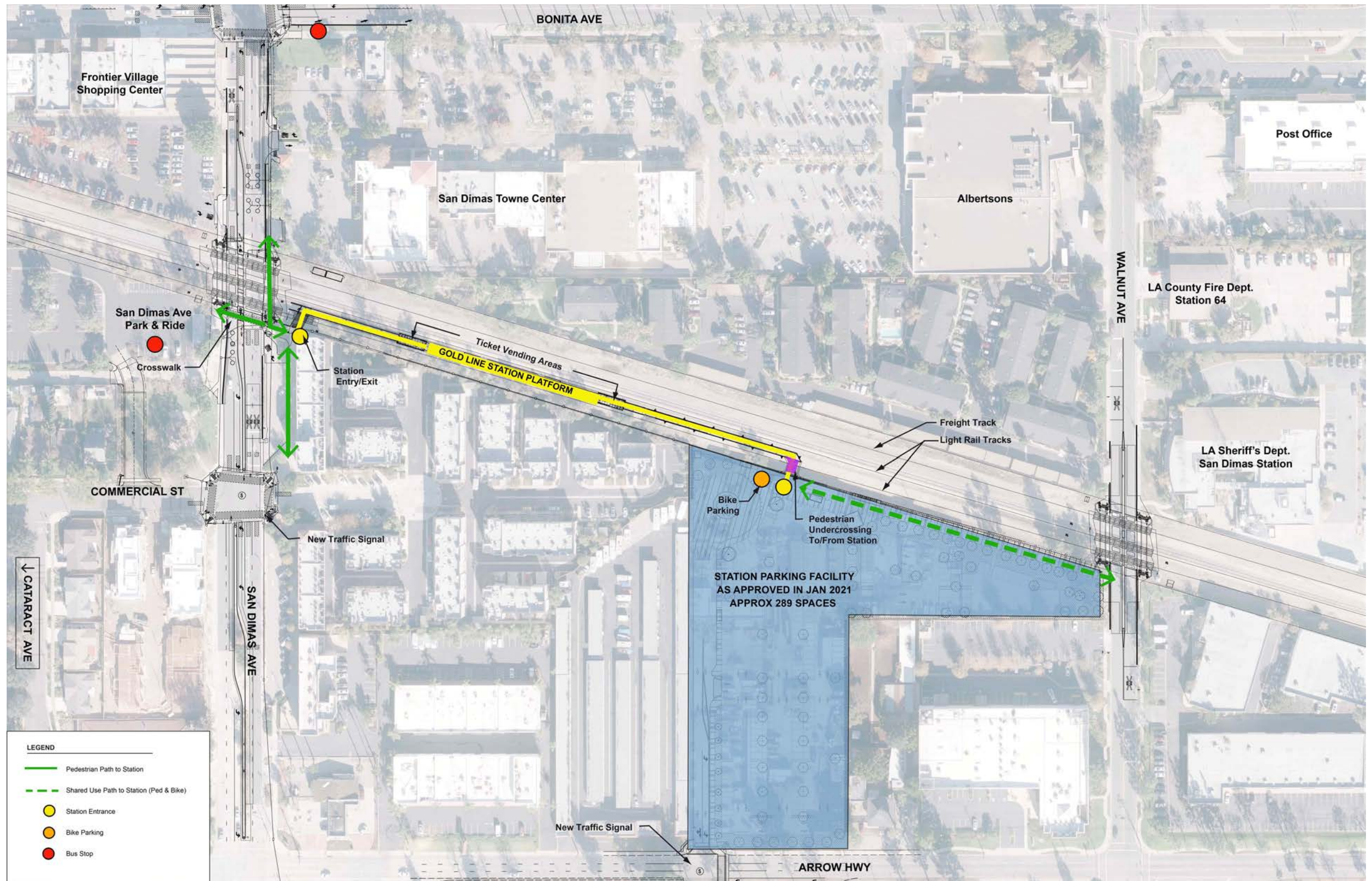


# Glendora Station Rendering





# San Dimas Station Site Plan



As approved in January 2021, the parking facility will have approximately 289 spaces.

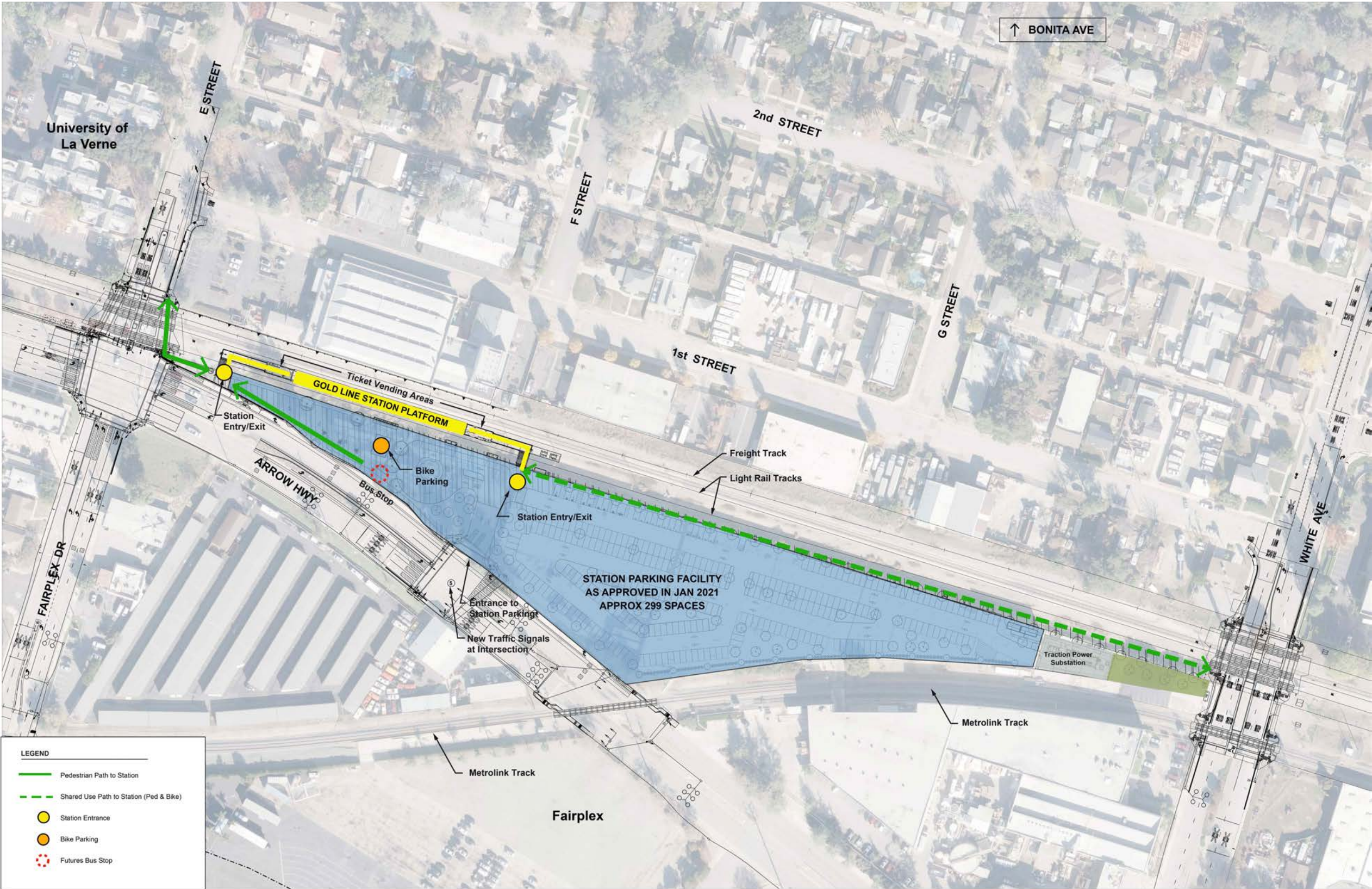


# San Dimas Station Rendering





# La Verne Station Site Plan



As approved in January 2021, the parking facility will have approximately 299 spaces.



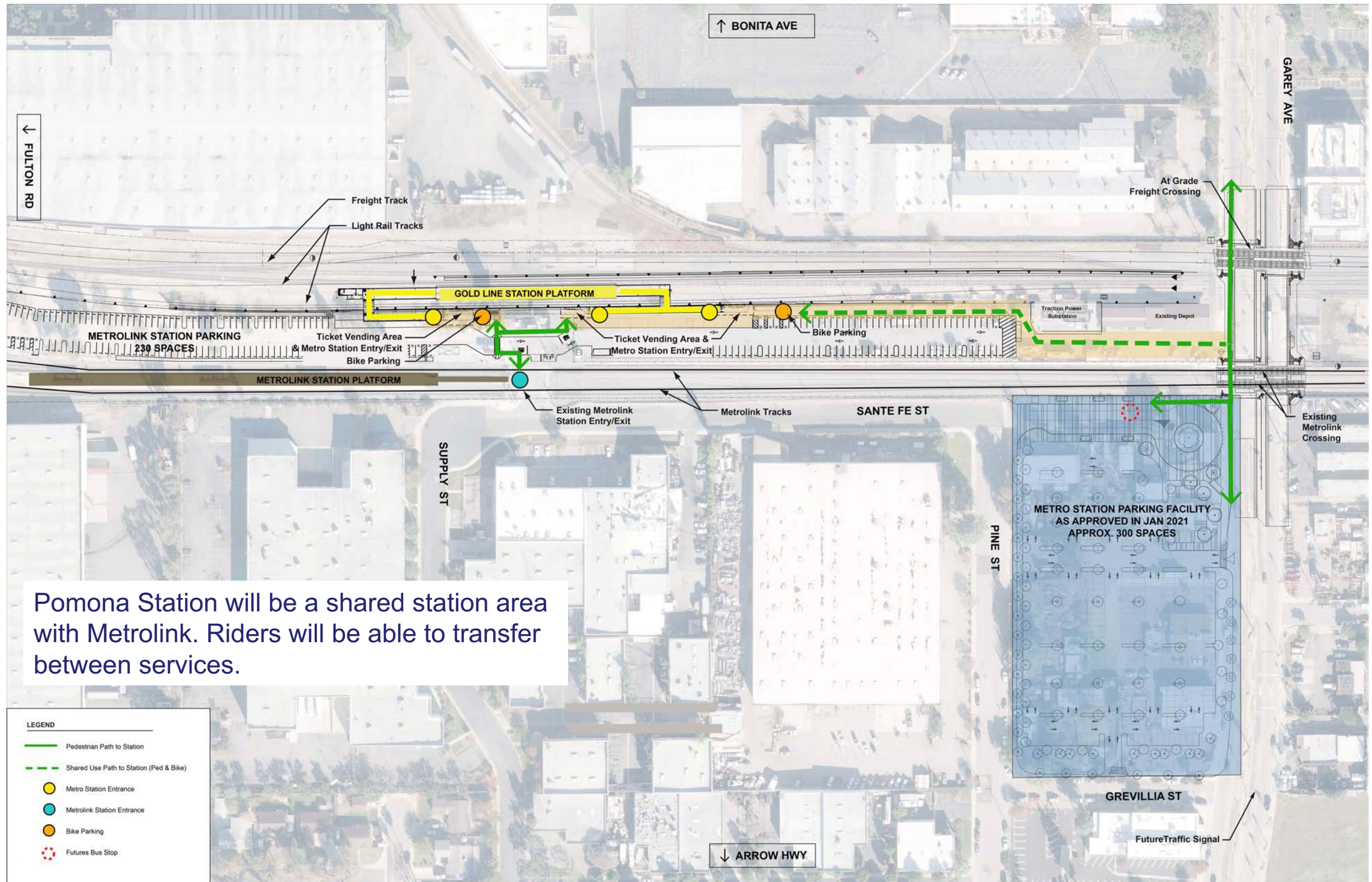
# La Verne Station Rendering



Foothill Gold Line



# Pomona Station Site Plan



Pomona Station will be a shared station area with Metrolink. Riders will be able to transfer between services.

As approved in January 2021, the total parking available at the station will be 530 spaces.



# Pomona Station Rendering

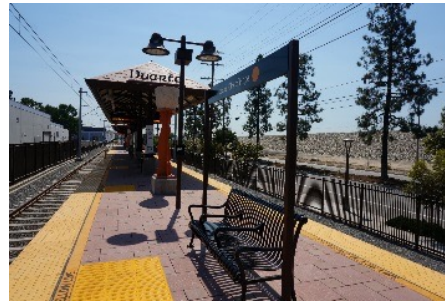


Foothill Gold Line

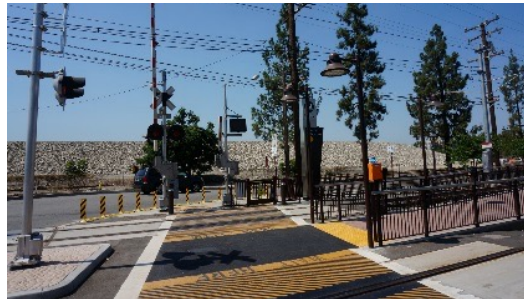


# Station Elements

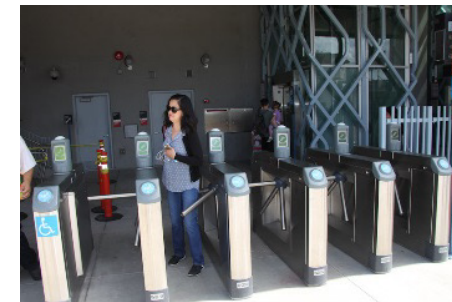
## Station Features



## Safety Elements



## Ticket Vending Area



## Systems Equipment





# Recently Approved Parking Modifications

- Reduced total parking spaces to be built at stations to meet Metro's parking demand forecast.
- Surface parking lots will be built instead of parking structures at the Glendora, San Dimas, La Verne and Pomona Stations.
- Expanded property will be needed for the Glendora and San Dimas Station parking facilities to accommodate parking in a surface lot.
- The Pomona Station parking facility location was changed.
- No changes were made to the location or footprint needed for the La Verne Station; or Montclair Station (Montclair is not part of funded project).
- Claremont Station will have a reduced sized parking structure (Claremont is not part of funded project).
- Changes were made to vehicular and pedestrian access and turnabouts.

NOTE: The above changes were approved in January 2021 as part of the Supplemental EIR process and were made in response to a Metro request that the project conform to their updated Supportive Transit Parking Program's forecast parking demand.





# Station Bicycle Parking

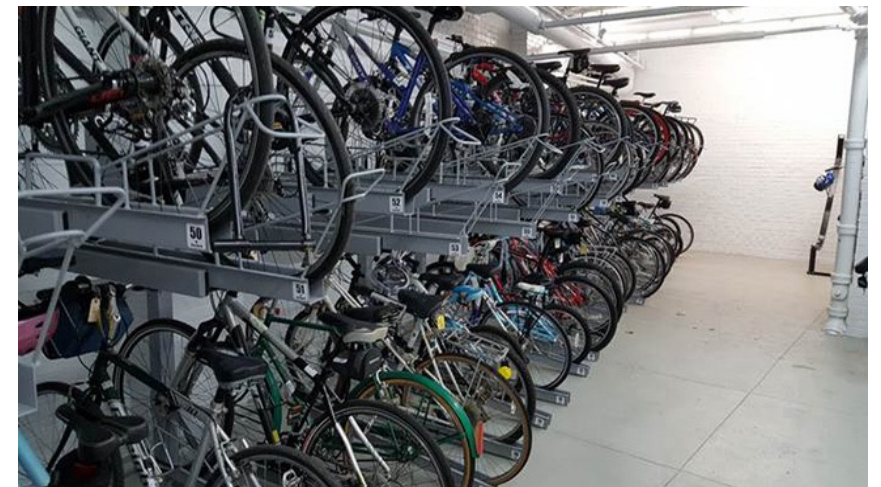
Each station will have bike parking:

- Glendora, San Dimas, Pomona Stations:  
32 spaces in shelter + 10 bike racks outside
- La Verne Station:  
64 spaces in shelter + 10 bike racks outside

Bike racks are outside and accessible to all.  
Provided under a canopy.

Bike shelters will use “EZ lift” racks. Shelter unit will be provided using wire mesh instead of glass panels for better air circulation.

The shelters will have an access panel similar to other secure Metro bike parking.





# Station Artwork

- Public art is an essential component of the station design.
- The Construction Authority launched the “**Station Design and Art Review Program**” (**SDAR**) in order to allow City staff and community representatives to work with the Construction Authority and project station artists to provide recommendations for the future station artwork.
- The station artwork concepts that were developed through the SDAR process were summarized and sent to city staff and each SDAR committee in January 2017.
- These artwork concepts were incorporated into the project's Design-Build package.
- In Fall 2019, the four station artists for the Glendora to Pomona segment were brought under contract with KPJV to develop their final design documents for the station artworks.
- The station construction documents for the art program are consistent with the themes and approach that each artist presented at the most recent community meetings that took place in early 2020.
- It is anticipated that fabrication of the station artwork will begin this Summer and be completed by Summer 2023.





# Glendora Artist Concept: “**GLENDORA HOME - A SLICE OF SUNSHINE**”

Station Artist — **MICHAEL HILLMAN**



*Glendora Home* refers to a brand or oranges that were grown and packed in Glendora.

*A Slice of Sunshine* refers to a marketing slogan used to promote the local orange industry.

The art and station design will draw from the history, culture and particularly the landscape of the area.

Image taken from a studio visit with Michael Hillman. The artist produced a series of sample hand-painted tiles as part of the design phase.



**Foothill Gold Line**



# Glendora Artist Concept: “**GLENDORA HOME -A SLICE OF SUNSHINE**”

Station Artist – **MICHAEL HILLMAN**



Samples images of 4 of the art columns. Each column concrete base is a light green color and incorporates a design motif to evoke the rolling hills of Glendora on the base.

The art column mosaics include the Moreton fig tree, lemon, brodeia filofia and the historic bougainvillea on Bennett Ave.

## **STATION ART COLUMNS:**

Eight columns will be created of precast concrete and Bisazza mosaic tile (4 columns on the platform and 2 columns that include benches at each station entrance station).

Each column will have a unique mosaic pattern that celebrates the flora or history of Glendora and will have a decorative capital inspired by the forms of citrus trees and classical column motifs.

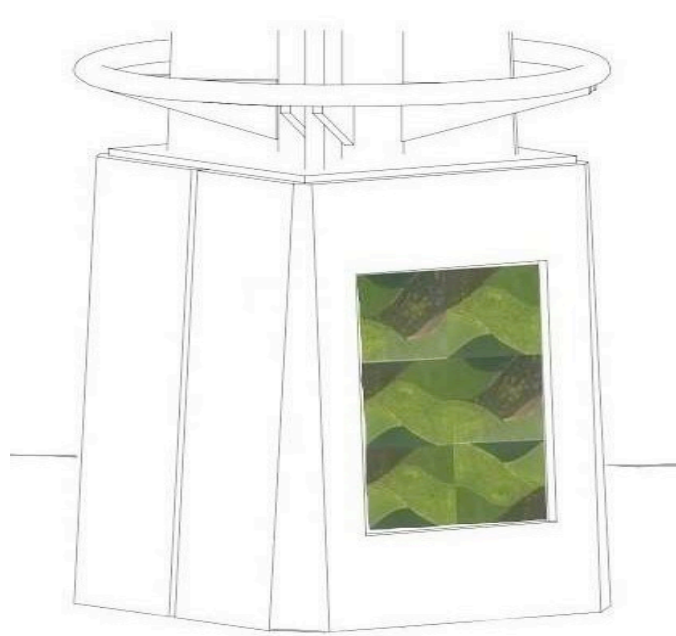


**Foothill Gold Line**

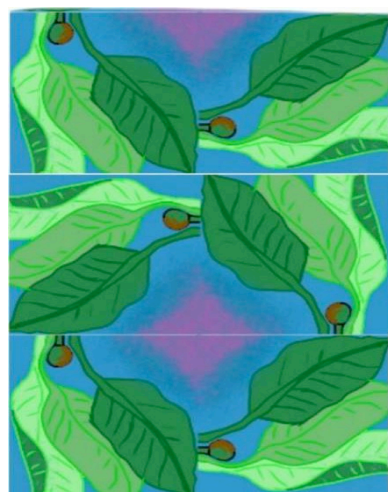
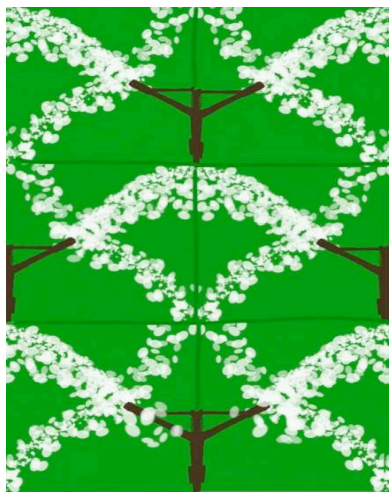


# Glendora Artist Concept: “**GLENDORA HOME - A SLICE OF SUNSHINE**”

Station Artist — **MICHAEL HILLMAN**



Details of four tile patterns. Some patterns may be repeated and the tile orientation changed to create a unique design.



## **HAND PAINTED TILES:**

The station platform canopy bases will be embellished with an inset of hand painted tiles. The tiles will be designed and glazed using a traditional technique called cuerda seca and installed in a tessellation, pinwheel, mirror and flipped formation.

The imagery is inspired by the vast foothills, the history of Glendora and the native plants from the foothills.



**Foothill Gold Line**



## San Dimas Artist Concept: “A SENSE OF PLACE”

### Station Artists — **EUGENE DAUB & ANNE OLSEN DAUB**



The Daub team views San Dimas as a unique town with a fascinating history nestled against the foothills of the San Gabriel mountains.

Many things about the area have changed since the advent of the railroad and the citrus industry, but not the natural beauty of the town and its setting.

The town has a specific sense of place born of a unique history of the land and the connection with flora, fauna, water and geography. There are no less than 25 parks and countless trails and hiking paths.

The artworks created for this station celebrate the wonders of the parks, mountains and local history.





# San Dimas Artist Concept: “A SENSE OF PLACE”

Station Artists — **EUGENE DAUB & ANNE OLSEN DAUB**



Station Platform Base

Above: Image showing rendering of bas-relief tiles to be inset into canopy column bases.

Right: Image of tile sample produced by the Daub team as part of the design process.



Below: watercolor sketches for the casted and glazed inset tile panels.



Foothill Gold Line



# San Dimas Artist Concept: “A SENSE OF PLACE”

Station Artists — **EUGENE DAUB & ANNE OLSEN DAUB**

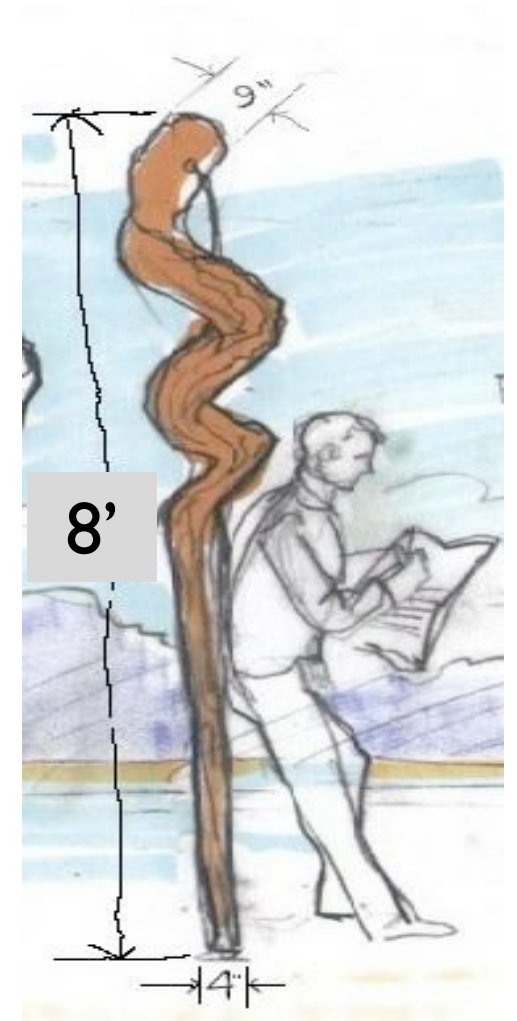


## WALKING STICKS:

Inspired by the animals and flora of San Dimas, the artist team will carve four oversized bronze casted sculptures that are influenced by the tradition of hand-carved and embellished walking sticks commonly used by hikers.



Design Inspiration:  
Walking sticks used by hikers



Foothill Gold Line



# La Verne Artist Concept: **“CONNECTIVITY”**

Station Artist — **BLUE McRIGHT**



Blue will create an artwork that consists of three sculptures that symbolize the connection between three important destinations near the station - Downtown La Verne, The University of La Verne and Fairplex.

The artworks are located in the station plaza, highly visible to both train riders, pedestrians and motorists on Arrow Hwy. They will visually link the platform to the plaza, engage viewers and become an identifying landmark at the La Verne Station.

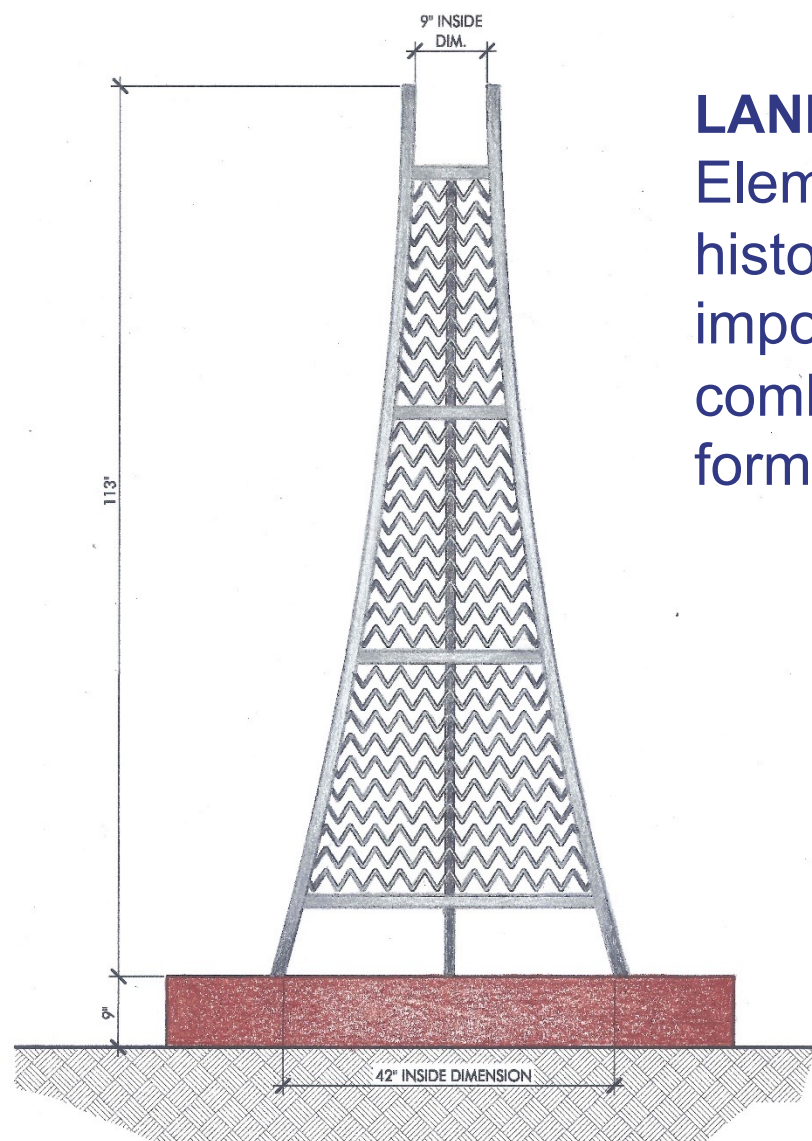


Foothill Gold Line



# La Verne Artist Concept: “CONNECTIVITY”

## Station Artist — **BLUE McRIGHT**



**LANDMARK ARTWORKS:**  
Elements central to the local history, citrus heritage and importance of water are combined in the sculptures' form.



Design Inspiration:  
Metropolitan Water District's Weymouth Tower



Design inspiration:  
Citrus ladder



**Foothill Gold Line**

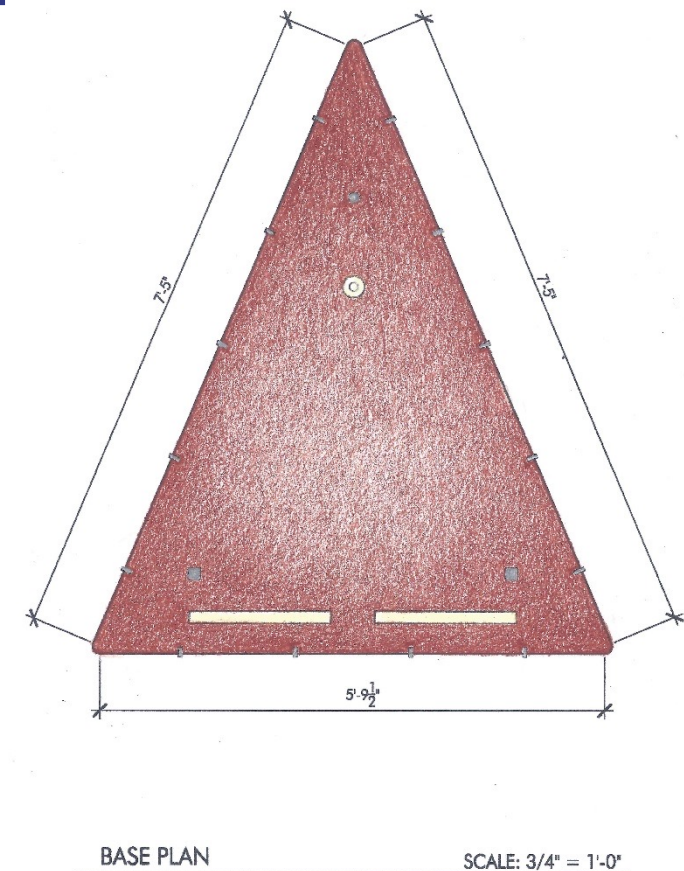
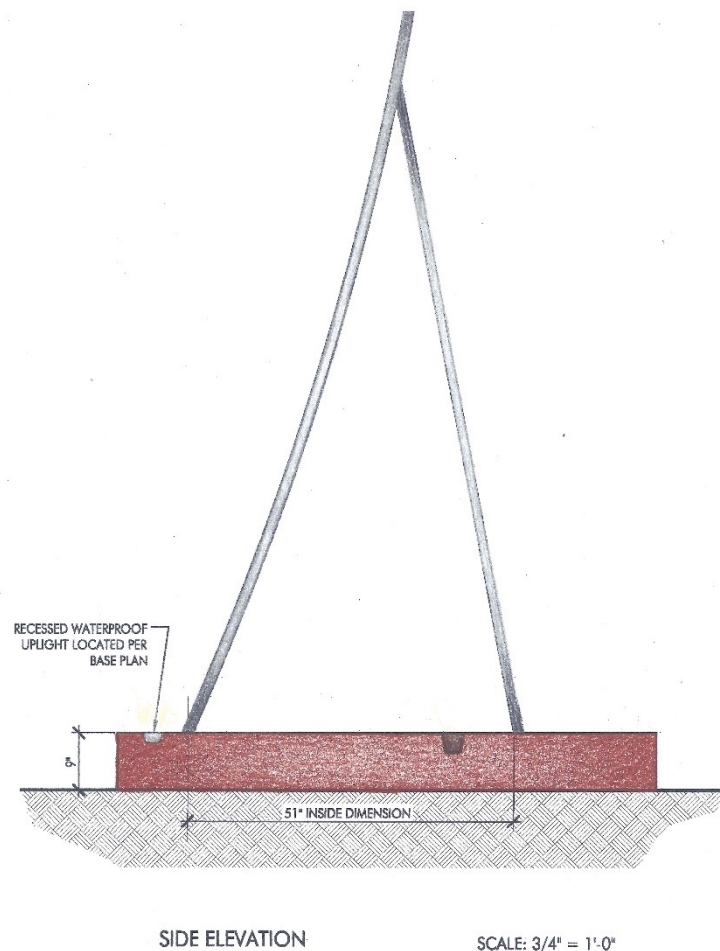


# La Verne Artist Concept: **“CONNECTIVITY”**

## Station Artist – **BLUE McRIGHT**

### LANDMARK ARTWORKS:

Each of the sculptures will point to one of the three important community partners – Downtown La Verne, The University of La Verne and Fairplex.



Images above show the side view and the colored concrete plinth that include embedded lights to illuminate the sculptures at night. The bead blast stainless steel finish of each sculpture will make each work a beacon of light.



**Foothill Gold Line**



# Pomona Artist Concept: **“THE POWER OF POMONA IS PEOPLE”**

## Station Artist — **STEPHEN FARLEY**



Stephen plans to honor inspiring Pomona citizens through his art.

With the help of various community programs and a general public call to all Pomona residents through a website and media outreach, people will be asked to submit a photo of the resident who inspired them and to share the story.

50-60 sets of portraits and quotes will be selected and featured on glazed tiles, using the tilography process. These images will be incorporated into ramps near the station platform.

His concept also includes the installation of large form lined embedded letters at the station entrance which spell out the project title, “The Power of Pomona a is People”





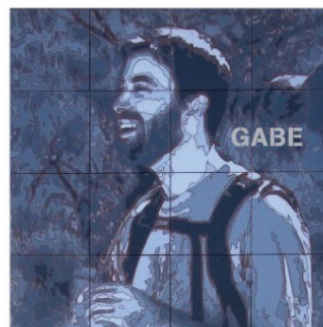
# Pomona Artist Concept: “THE POWER OF POMONA IS PEOPLE”

## Station Artist – **STEPHEN FARLEY**



### HALL OF GRATITUDE:

The station will have 50-60 pairs of portraits and quotes that highlight the people of Pomona.



The project received more than 100 submissions. Portrait images and stories are reviewed and posted on the HALL OF GRATITUDE website: [www.pomonapeoplepower.com](http://www.pomonapeoplepower.com)





# Pomona Artist Concept: **“THE POWER OF POMONA IS PEOPLE”**

Station Artist — **STEPHEN FARLEY**



The image above shows the pairs of portraits and quotes that will be incorporated into the station railing as well as the concrete form-lined sign that highlights the project name. This concrete wall will have low landscape and lighting to illuminate the wall at night.





# **Freight Track Relocation**

**Chris Burner**

**Construction Authority**

**Chief Project Officer**



# Shared Corridor Requires Relocation

- The entire segment from Glendora to Pomona has an operational freight track throughout.
- BNSF operates the freight service.
- The rail corridor is being modified to accommodate three tracks: one freight track and two light rail tracks.
- The freight track is currently positioned in the center portion of the rail corridor and must be moved to one side to accommodate the two additional light rail tracks.





# Shared Corridor

- KPJV has created a phasing plan for relocating the freight track.
- KPJV has coordinated with BNSF and SCRRA to relocate and rebuild many segments of the freight track while temporarily out of service.
- First of three freight rail shifts - to relocate track from Barranca Ave to Gladstone St. (Glendora) - to be completed by June 1, 2021.
- Two more shifts to occur by end of year.





# Operating Parameters

- BNSF (Freight) trains will have max speed of 40 mph
- Gold Line (Light Rail) trains will have max speed of 55 mph
- New train control systems will be installed for both LRT and freight track
- Tracks will share at-grade crossings, except for grade-separated light rail at Grand Avenue, Lone Hill Avenue, and Bonita Avenue/Cataract Avenue.



Foothill Gold Line



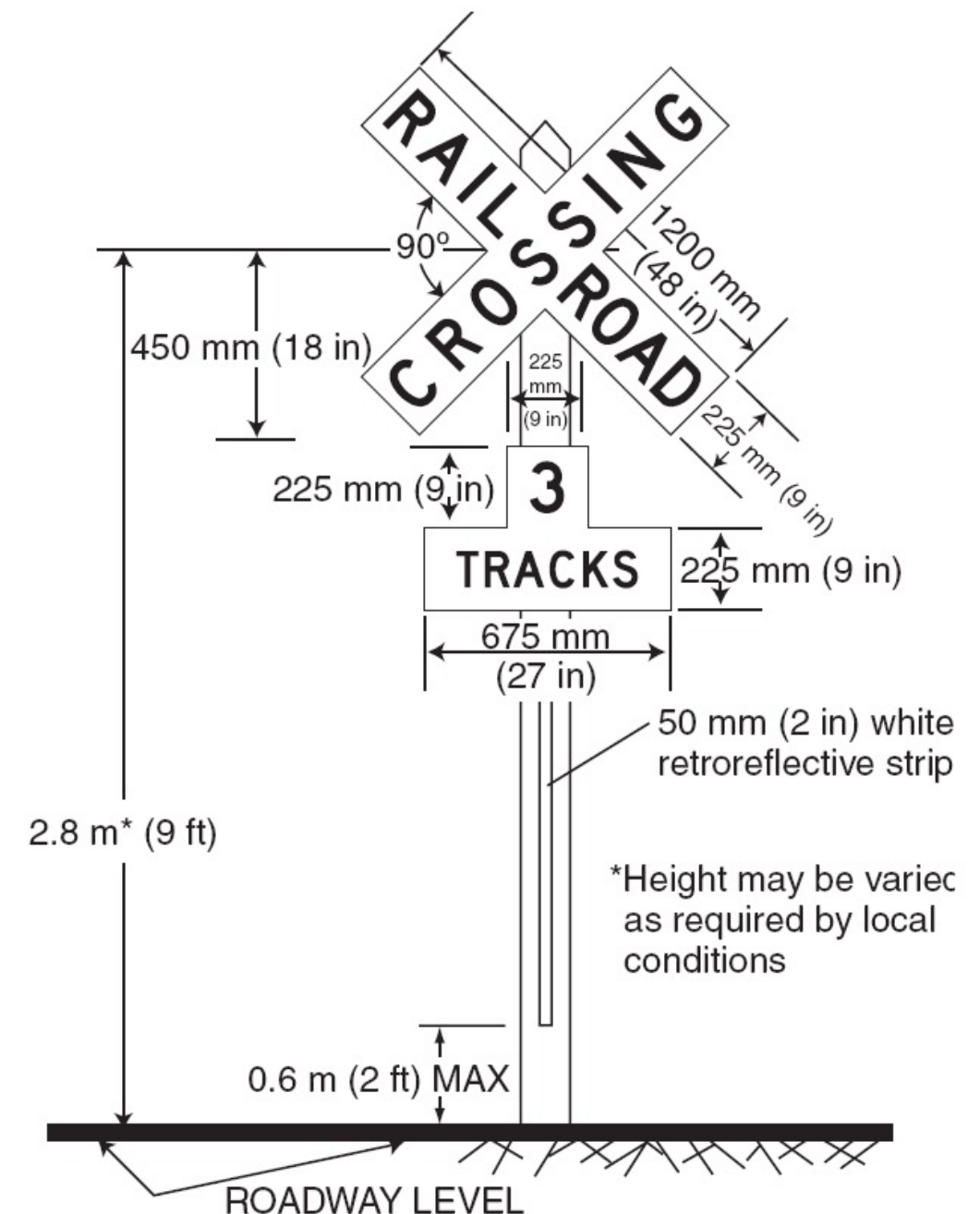


# Grade Crossings



# Grade Crossings Types

- Grade Crossings
  - Types
    - At-grade crossing: tracks intersect with streets/roads
    - Grade separated crossing: tracks do not intersect with streets/roads
    - Pedestrian crossings: points at which pedestrians cross tracks
  - Grade crossings are governed by the California Public Utility Commission (CPUC)





# Plan for Major Street Crossings

MAJOR STREET CROSSINGS	At Grade Crossing		Bridge	
	Gold Line	Freight	Gold Line	Freight
Barranca Ave	X	X		
Foothill Blvd/Grand Ave		X	X	
Vermont Ave/Ada Ave (permanent closure of Ada)	X	X		
Glendora Ave	X	X		
Pasadena Ave	X	X		
Glenwood Ave (permanent closure at railroad tracks)				
Elwood Ave	X	X		
Lorraine Ave	X	X		
Route 66			X	
Lone Hill Ave		X	X	
Gladstone St	X	X		
Eucla Ave	X	X		
Bonita Ave/ Cataract Ave		X	X	
Monte Vista St (permanent closure for vehicles, ped access to be provided)				
San Dimas Ave	X	X		
Walnut Ave	X	X		
San Dimas Canyon Rd	X	X		
Wheeler Ave	X	X		
A St	X	X		
D St	X	X		
E St	X	X		
White Ave	X	X		
Fulton Ave (new turn restrictions)	X	X		
Garey Ave		X		

21 at-grade crossings and 19 bridges will be built as part of the project



**Foothill Gold Line**



# Safety at At-Grade Crossings



## Safety Equipment at Shared At-Grade Crossings:

- Exit Gates (Quad Gates) for vehicles and pedestrians
- Raised Medians (approximately 100 feet on either side of tracks)
- New Red Curbs (approximately 100 feet on all curbs)
- Turn restrictions from driveways within 100 feet of tracks
- Look Both Ways-LED signals

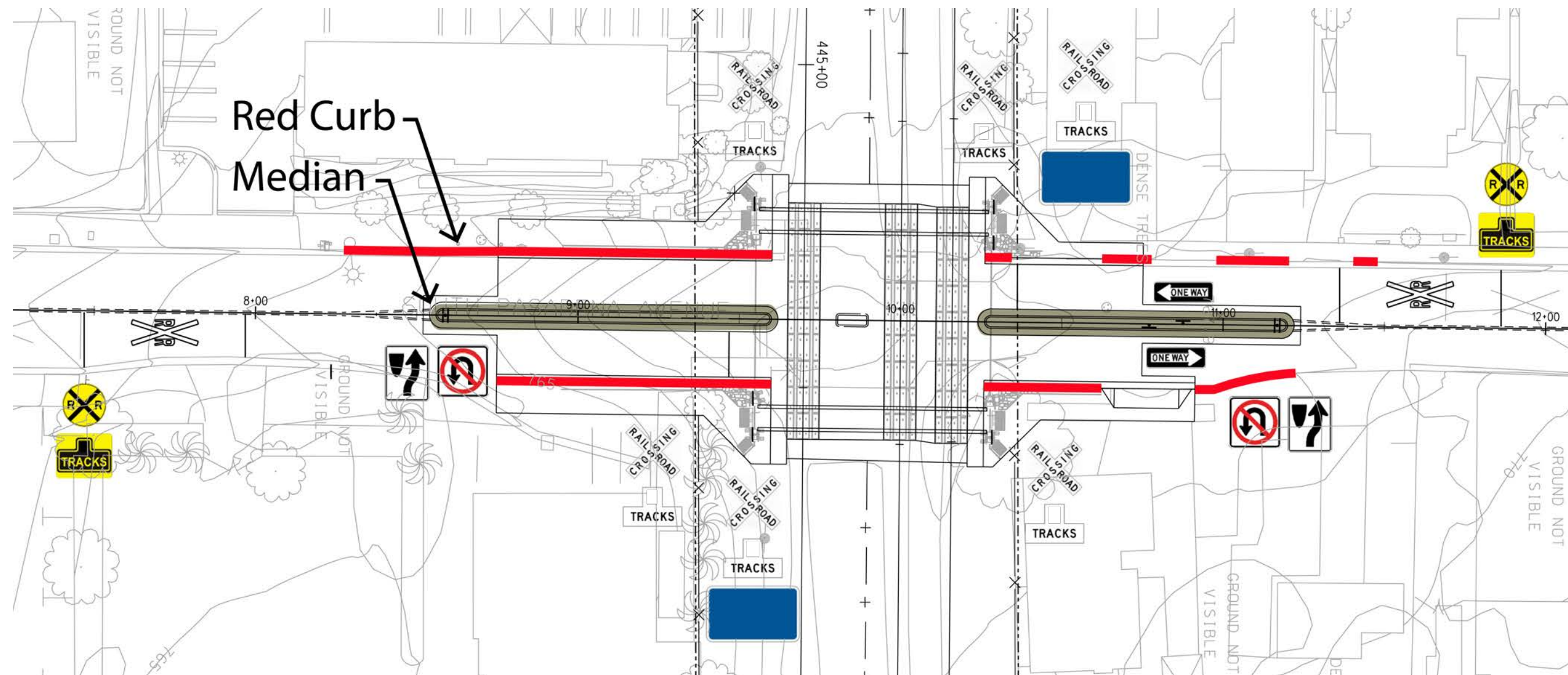




# At-Grade Crossings – Street Changes

## New Medians; Red Curbing; Turn Restrictions (within 100 ft.)

- New safety features are required by the California Public Utilities Commission
- All crossings are uniquely designed, but will include similar measures





# Grade Separated Crossings - Bridges

- Glendora to Pomona segment contains a total of 19 bridge structures that will be modified, replaced or new
  - 10 light rail crossings
  - Seven (7) freight crossings
  - Two (2) retaining wall structures underneath existing bridges
- Of these 19 bridge structures:
  - 17 will be new
  - One (1) will be total replacement
  - One (1) will be modification of an existing structure





# New Structures

New structures will be built where a structure does not currently exist

1. Foothill & Grand Grade Separation
2. Little Dalton Wash Bridge (FRT)
3. Big Dalton Wash Channel
4. Big Dalton Wash Channel (FRT)
5. Route 66 Grade Separation
6. Retaining Walls - East Glendora Overhead at Route 210
7. San Dimas Wash Channel Bridge
8. Lone Hill Ave Flyover
9. Retaining Wall - Gladstone Overhead at State Route 57
10. Bonita & Cataract Grade Separation
11. Puddingstone Channel Bridge
12. Puddingstone Channel Bridge (FRT)
13. Marshall Creek Culvert Protection
14. Marshall Creek Bridge (FRT)
15. Live Oak Wash Channel Bridge
16. Live Oak Wash Channel Bridge (FRT)
17. Thompson Creek Channel Bridge (FRT)

NOTE: The highlighted bridges are for light rail trains only and will cross major city streets. Each was mandated by the California Public Utilities Commission for safety.





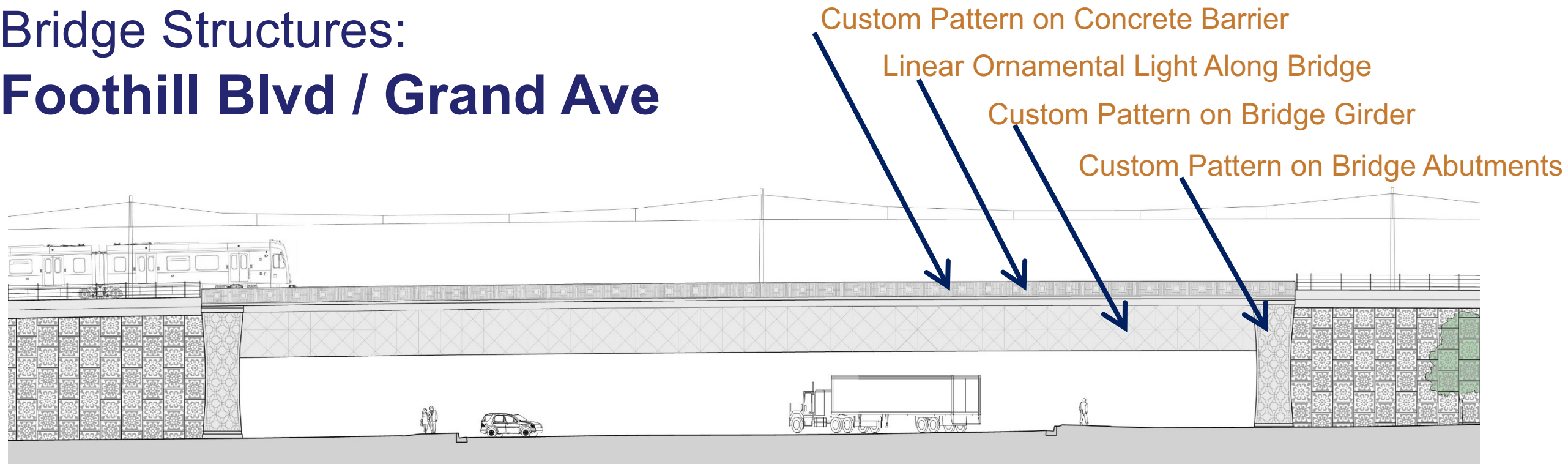
# Bridge Design Criteria

- All bridge designs are in accordance with applicable Governmental Rules/Agencies:
  - Metro Rail Transit Design Criteria
  - Metro Standard Drawings
  - SCRRA Standards
  - Caltrans Standard Specifications
  - Army Corps of Engineers Standards
  - AREMA code and freight railroad requirements
  - Los Angeles County Flood Control District



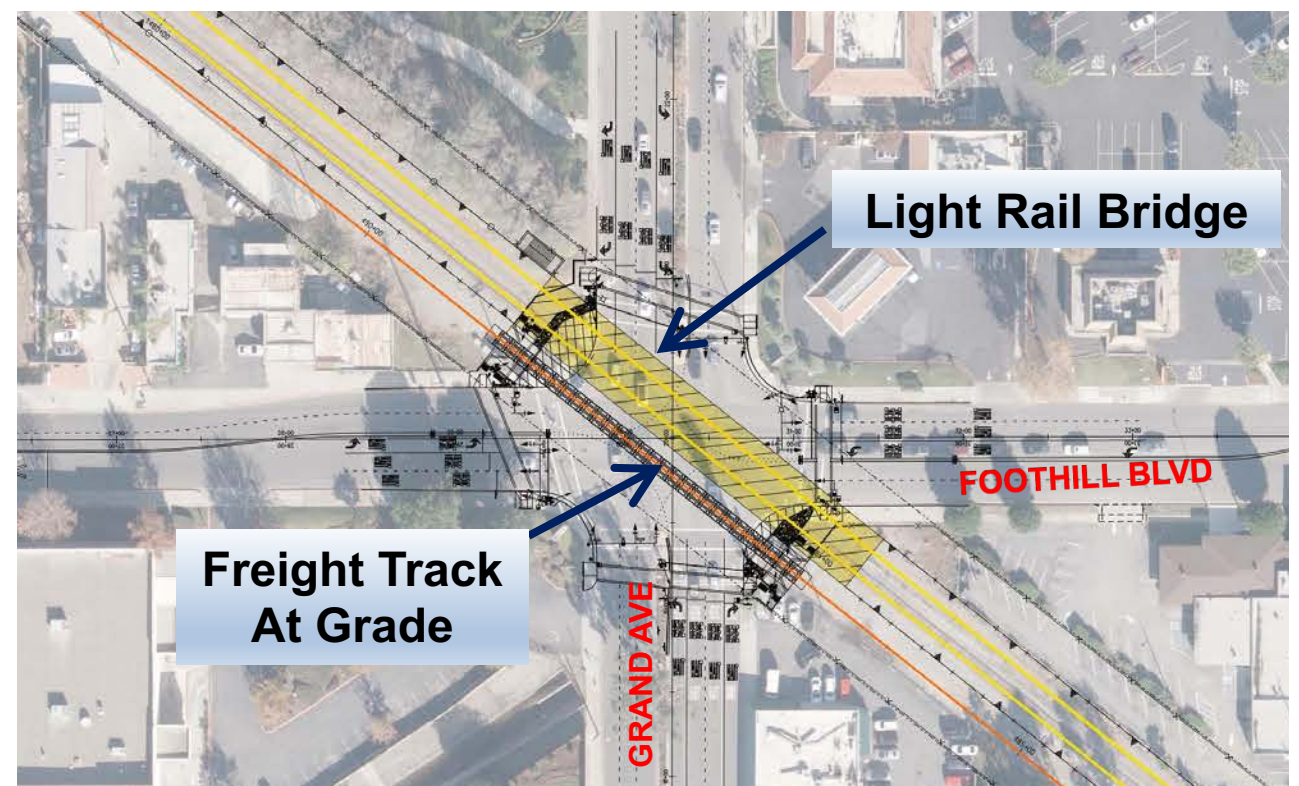


# Bridge Structures: Foothill Blvd / Grand Ave



## FOOTHILL BLVD / GRAND AVE ROADWAY MODIFICATIONS

- New Light Rail Bridge
- Freight Remains At Grade
- Modified Street Medians
- Modified Traffic Signal





# Grand Ave View, Looking North



**AECOM**

FOOTHILL GRAND - VISUAL SIMULATION 2  
MAY 19, 2021

Simulation is based on bridge design as of May 2021 and is not final.



**Foothill Gold Line**



# Foothill Blvd View, Looking East



**AECOM**

FOOTHILL GRAND - VISUAL SIMULATION 1  
MAY 19. 2021

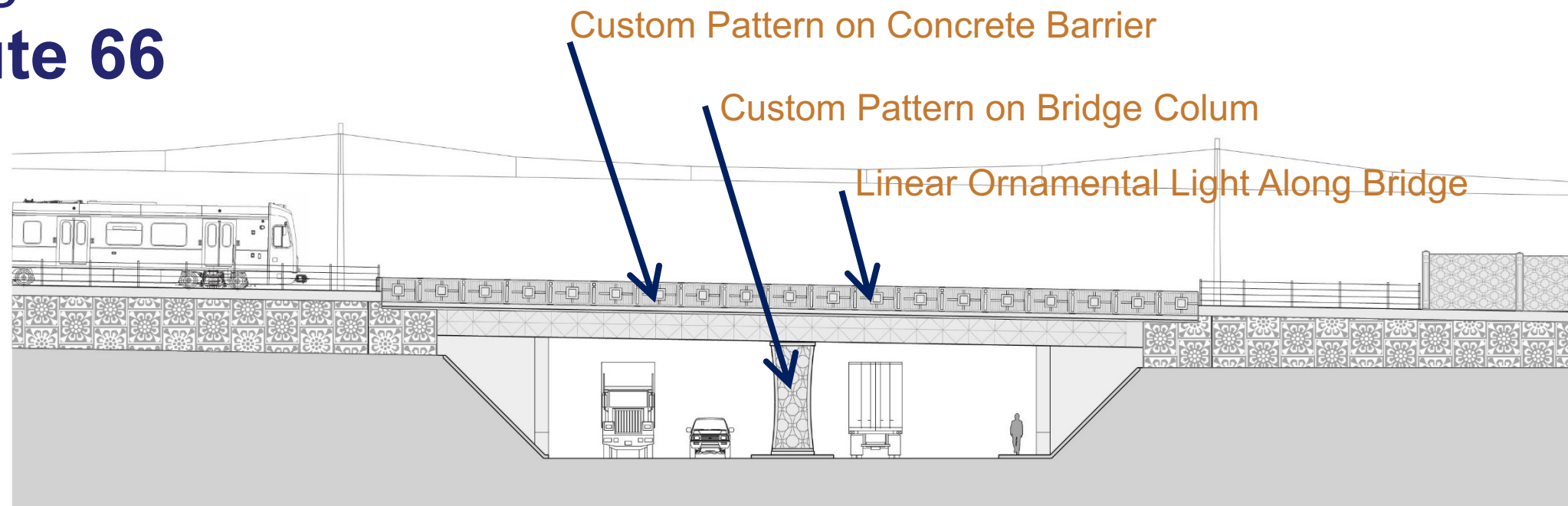
Simulation is based on bridge design as of May 2021 and is not final.



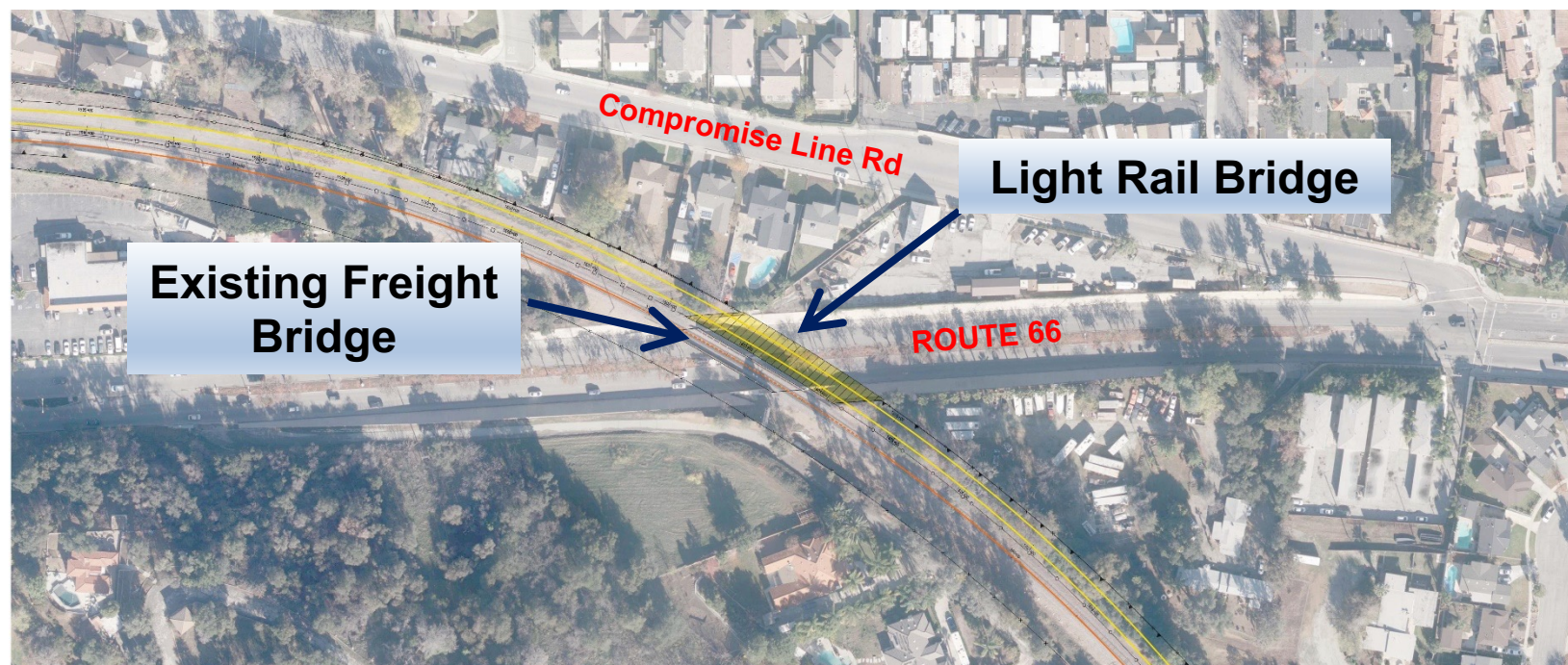
**Foothill Gold Line**



# Bridge Structures: Route 66

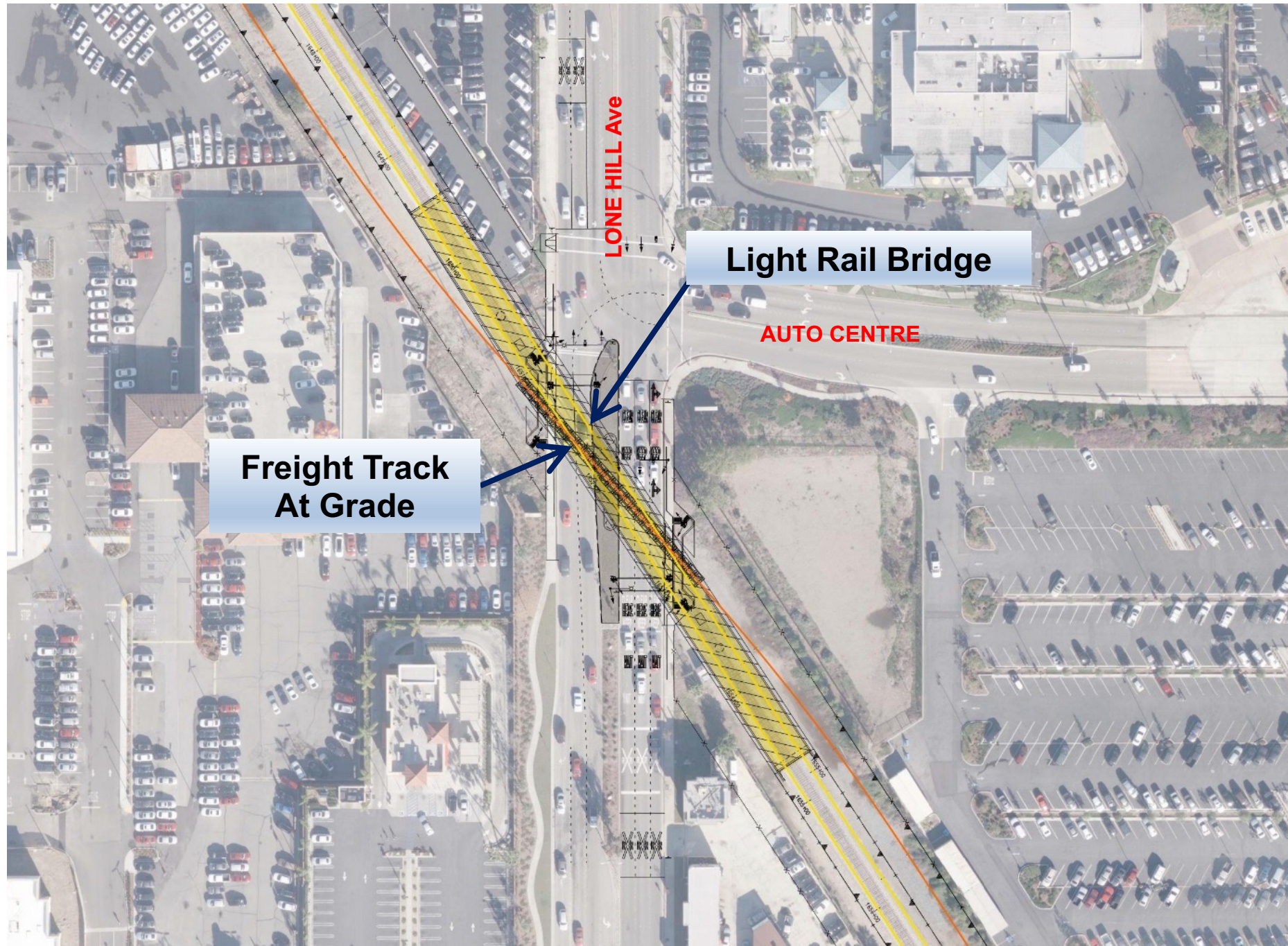


Elevation View Looking West





# Lone Hill Ave Roadway Modifications



## LONE HILL ROADWAY MODIFICATIONS

- New Light Rail Bridge
- Freight Remains At Grade
- Modified Street Medians
- Modified Traffic Signal



**Foothill Gold Line**



# Lone Hill Ave View, Looking North



**AECOM**

LONE HILL - VISUAL SIMULATION 1  
MAY 19, 2021

Simulation is based on bridge design as of May 2021 and is not final.



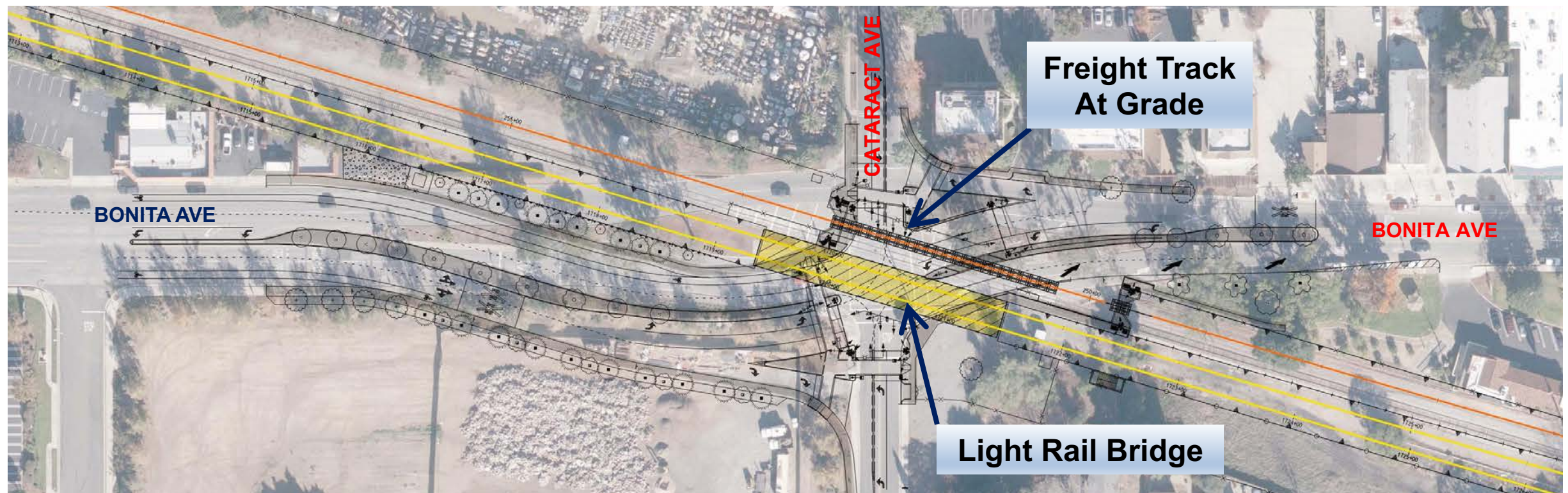
**Foothill Gold Line**



# Bridge Structures: Bonita Ave / Cataract

## BONITA / CATARACT ROADWAY REALIGNMENT

- New Light Rail Bridge
- Freight Remains At Grade
- Realigned Roadway and Modified Street Medians
- New Signalized Intersection

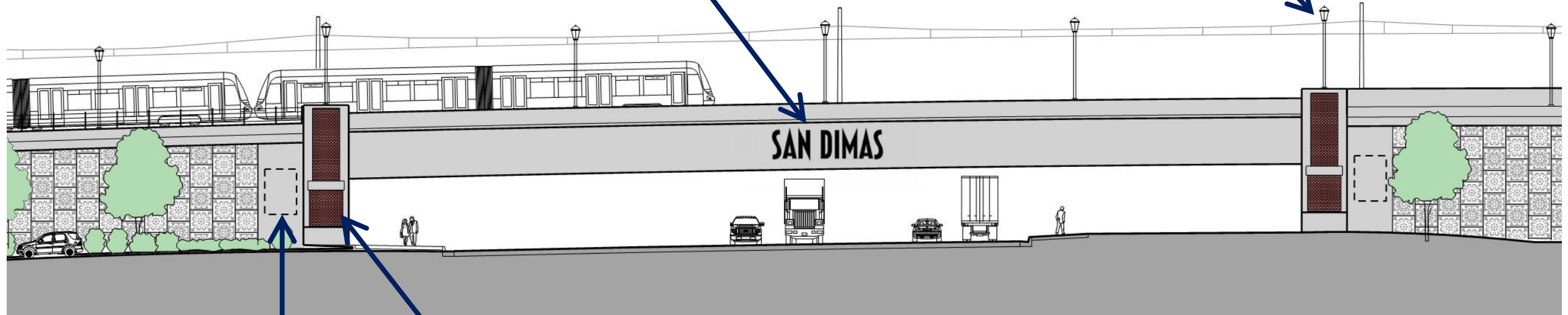




# Bridge Structures: Bonita Ave / Cataract Ave

Metal Sign each side of  
Bridge with Lighting

Bonita Ave  
Ornamental Light  
(see photo)



Brick Veneer on Bridge Abutment

Recessed Area for Artwork Graphic  
(provided by City)



Foothill Gold Line



# Bonita Ave View, Looking East



**AECOM**

BONITA CATARACT - VISUAL SIMULATION 1  
MAY 19, 2021

Simulation is based on bridge design as of May 2021 and is not final.



**Foothill Gold Line**



# Bonita Ave View, Looking West



**AECOM**

BONITA CATARACT - VISUAL SIMULATION 2  
MAY 19, 2021

Simulation is based on bridge design as of May 2021 and is not final.



**Foothill Gold Line**





# Fencing & Walls



# Corridor Fencing

## Metro Standard Wire Mesh



Foothill Gold Line



# Fencing at Stations

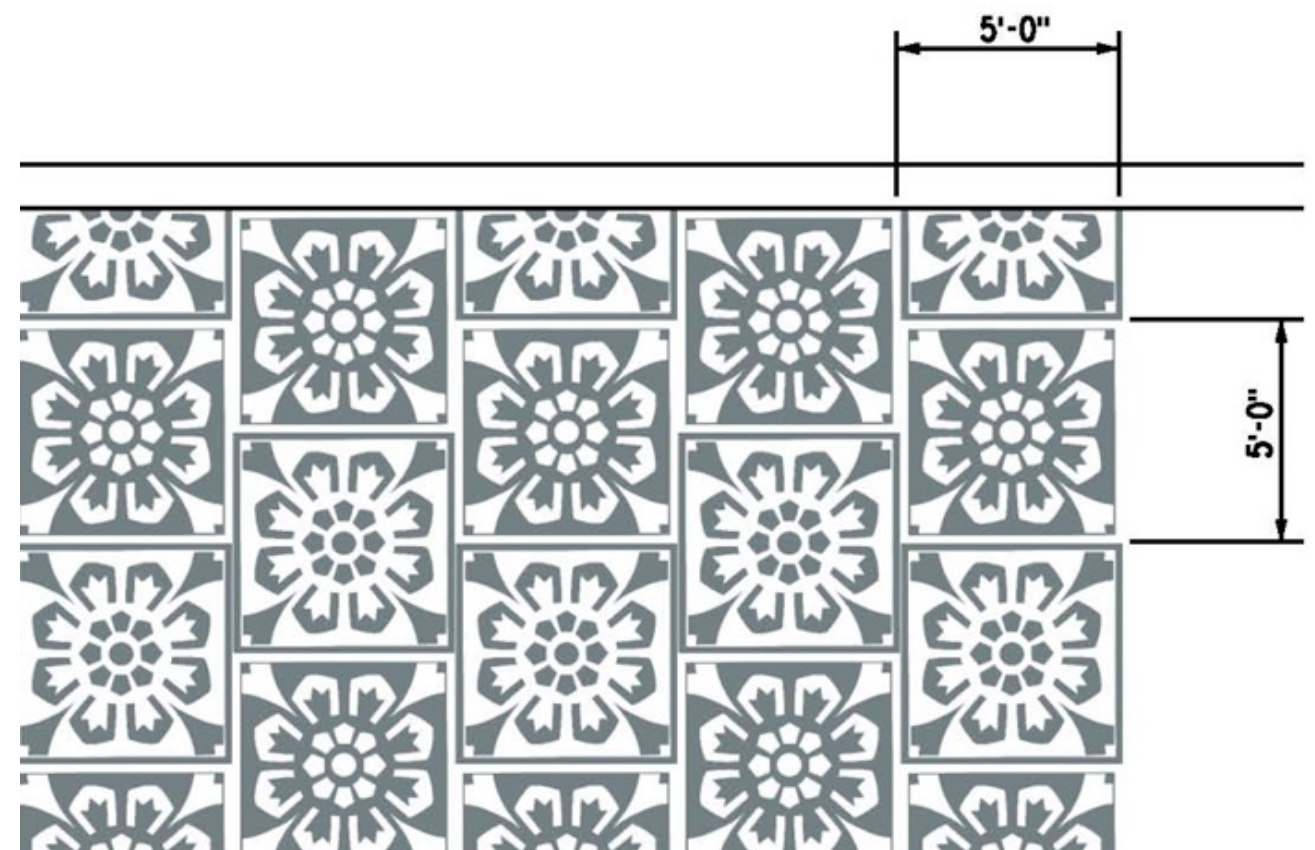
## Metro Standard Picket at Station Areas





# Retaining Walls

As part of the project, more than 24,000 linear feet of concrete retaining walls will be built. Design elements will be integrated in when these walls are visible to provide consistency throughout the project corridor and interest.



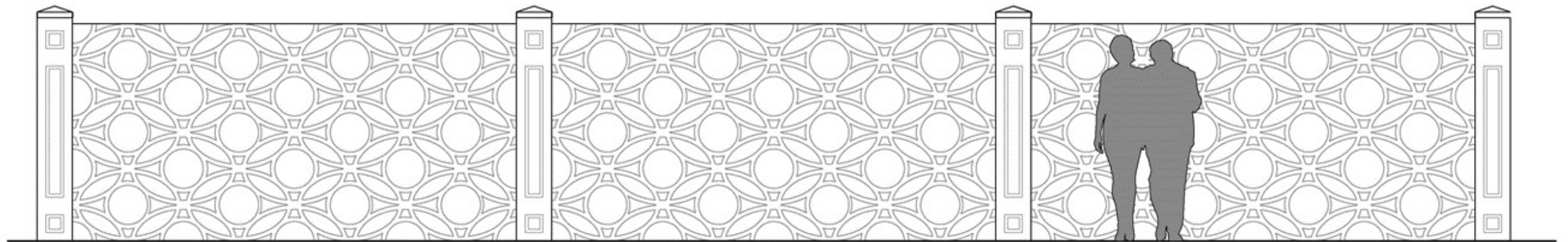
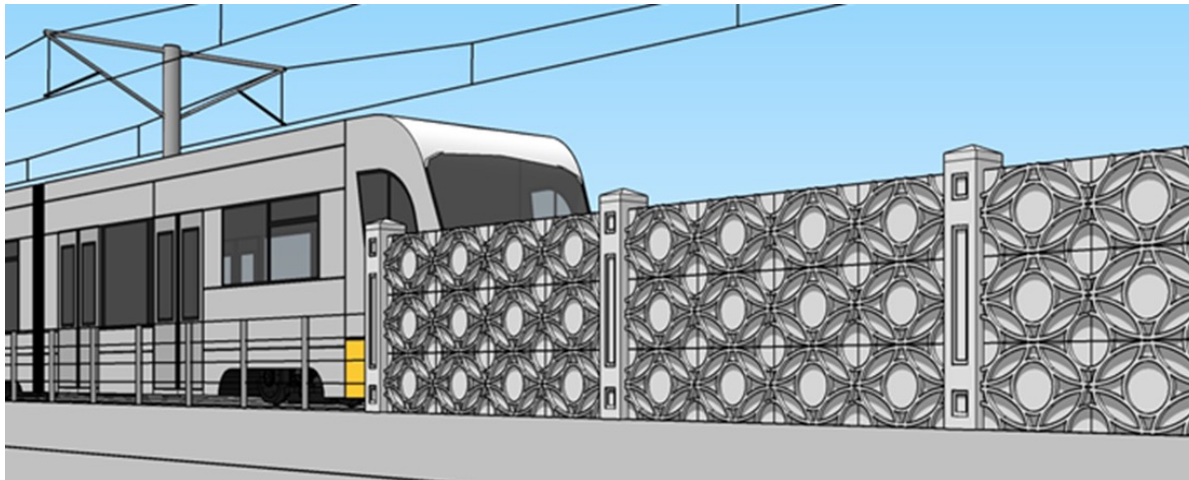
Wall elevation – MSE Retaining Walls





# Sound walls

- Noise & vibration studies determined sound wall locations & heights based on FTA impact criteria
- Sound wall height varies from 3'-7" to 17'(from top of rail)
- Sound walls to be located approx 10' from center of track or at the right-of-way line
- Approximately 27,600 feet of sound wall will be built along the alignment from Glendora to Pomona







# **Construction Status**

**Jeff Rowland**

**Kiewit-Parsons, a Joint Venture  
Community Relations Manager**



# Major Construction Began in July 2020



Foothill Gold Line



# 21 At-Grade Crossings to be Reconstructed



- Reconstruction requiring long-term street closures is complete at six of the 21 at-grade crossings
- Reconstruction is currently underway on five more





# Utility Relocation and Protection



At each at-grade crossing and throughout the 9.1-mile corridor, utilities that cross the tracks are being relocated or protected by KPJV and third-party utilities (water, sewer, communication conduit, electrical, etc.).



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# Bridgework Underway



Crews are underway constructing or renovating many of the 19 bridges for the project. Underway now are freight and light rail bridges over local water channels: Big Dalton Wash and San Dimas Channel Wash (Glendora), Puddingstone Channel (La Verne), and Thompson Creek (Pomona).

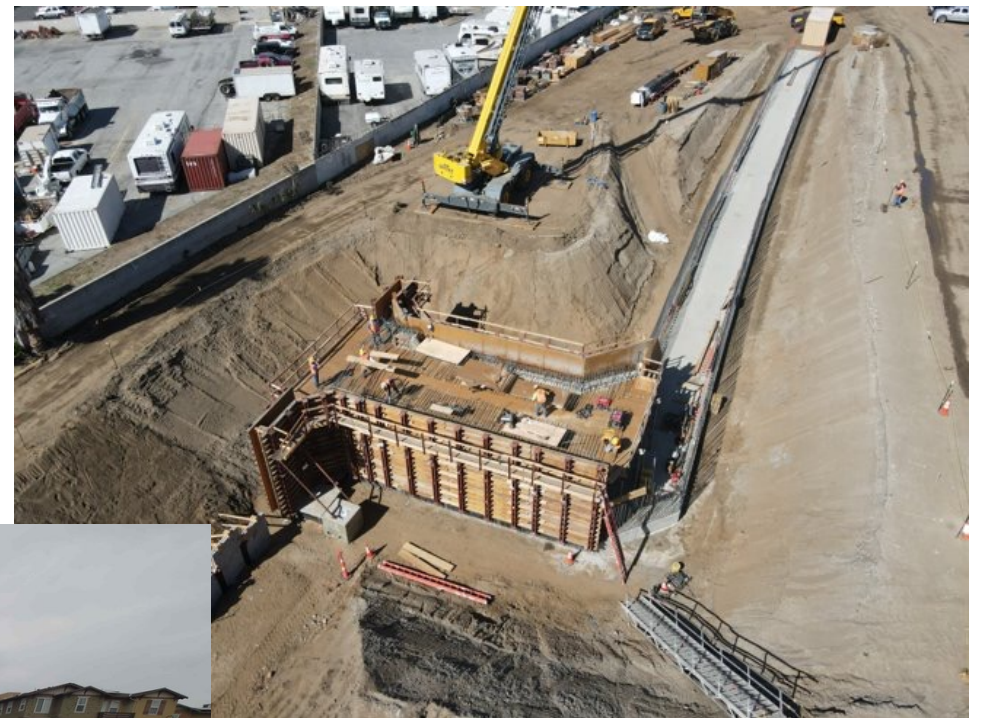
Initial work (such as utility relocation) has started on three of the four new light rail bridges to be built over major streets along the project route; each required by the California Public Utilities Commission for safety.



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# Glendora Station Underpass Underway



Crews have excavated for a pedestrian underpass at the future Glendora Station. The ramp and structure shown here will allow future riders to access the station platform from Glendora Ave and the future station parking facility by walking under the relocated freight and eastbound light rail track.



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# Freight Track Removal and Replacement



Crews are utilizing a track outage to remove the old freight track and rebuild it in its new relocated position between APU/Citrus College Station and Gladstone St in Glendora. This first section is anticipated to be fully complete this month.

The entire freight line will be relocated in three shifts; anticipated to be complete by the end of 2021.



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May 18, 2021  
Lisa Levy Buch  
Chief Communications and Strategic  
Development Officer  
(626) 305-7004 and (909) 267-0161  
[llevybuch@foothillgoldline.org](mailto:llevybuch@foothillgoldline.org)  
FOR IMMEDIATE RELEASE

### **Media Advisory**

## **Freight Train Service to Resume between Irwindale and Pomona as Early as June 1**

*Current freight track relocation nearing completion as part of Foothill Gold Line project*

### **WHO:**

Residents / Businesses / Local Stakeholders in cities of Irwindale, Azusa, Glendora, San Dimas, La Verne and Pomona

### **WHAT:**

**As early as June 1, 2021**, the BNSF freight trains will resume service in the rail corridor from Irwindale to Pomona. This will follow the completion of the freight track relocation in the city of Glendora, as part of the 9.1-mile, four-station Foothill Gold Line light rail project from Glendora to Pomona. The freight track relocation is necessary to make room in the corridor for two new light rail tracks for the future Gold Line trains.

The design-builder, Kiewit-Parsons JV (KPJV), has been accelerating the relocation of the freight track within the city of Glendora as a result of an agreement that started in November 2020 to suspend freight service temporarily through the city through May 2021. Crews are nearing completion on the relocation of approximately five miles of freight track between Barranca Ave. and Gladstone St. in Glendora. This accounts for the first of three portions of the project that will undergo relocation. Once freight service resumes, the freight trains will utilize the newly relocated freight track in Glendora.

### **WHEN & WHERE:**

Freight trains to resume service between Irwindale to Pomona along rail corridor as early June 1, 2021.

### **WHAT TO EXPECT:**

- Freight trains will resume service as early as June 1, 2021.
- Stay alert for trains.
- The freight train's horn will be sounded as it approaches each street crossing.

### **CONTACT INFORMATION FOR GENERAL OR CONSTRUCTION-RELATED QUESTIONS:**

Residents and businesses can get their project or construction-related questions answered via phone or email:

#### **Project Construction Questions:**

**Hotline:** (626) 513-5788

**Email:** [CommunityRelations@Kiewit-Parsons.com](mailto:CommunityRelations@Kiewit-Parsons.com)

#### **General Project Questions:**

**Call:** (626) 471-9050

**Email:** [PublicAffairs@Foothillgoldline.org](mailto:PublicAffairs@Foothillgoldline.org)

### **FOR MORE INFORMATION:**

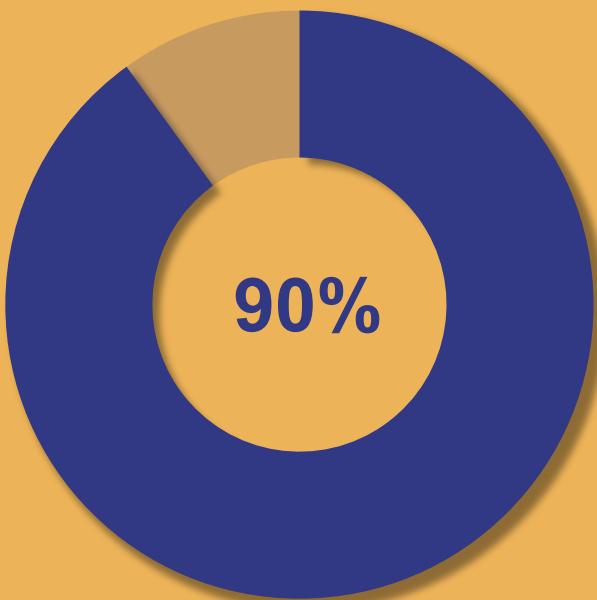
To learn of current or upcoming construction activities, visit the interactive construction map page at [www.foothillgoldline.org](http://www.foothillgoldline.org)



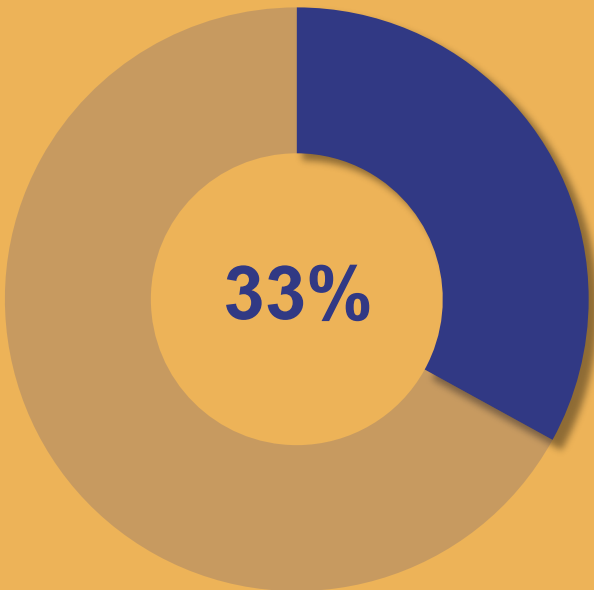


# Alignment Project Progress Milestones

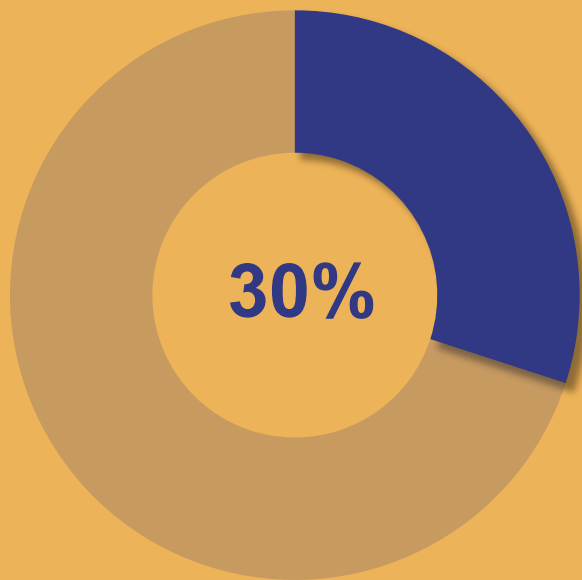
Major Milestones (% Complete)



Design



Freight Track Relocation



Grade Crossing Reconstruction



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# Alignment Project Progress Milestones

Overall Project Completion  
(% Complete)

33%



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# **Project Schedule**



# Alignment Project Schedule Update

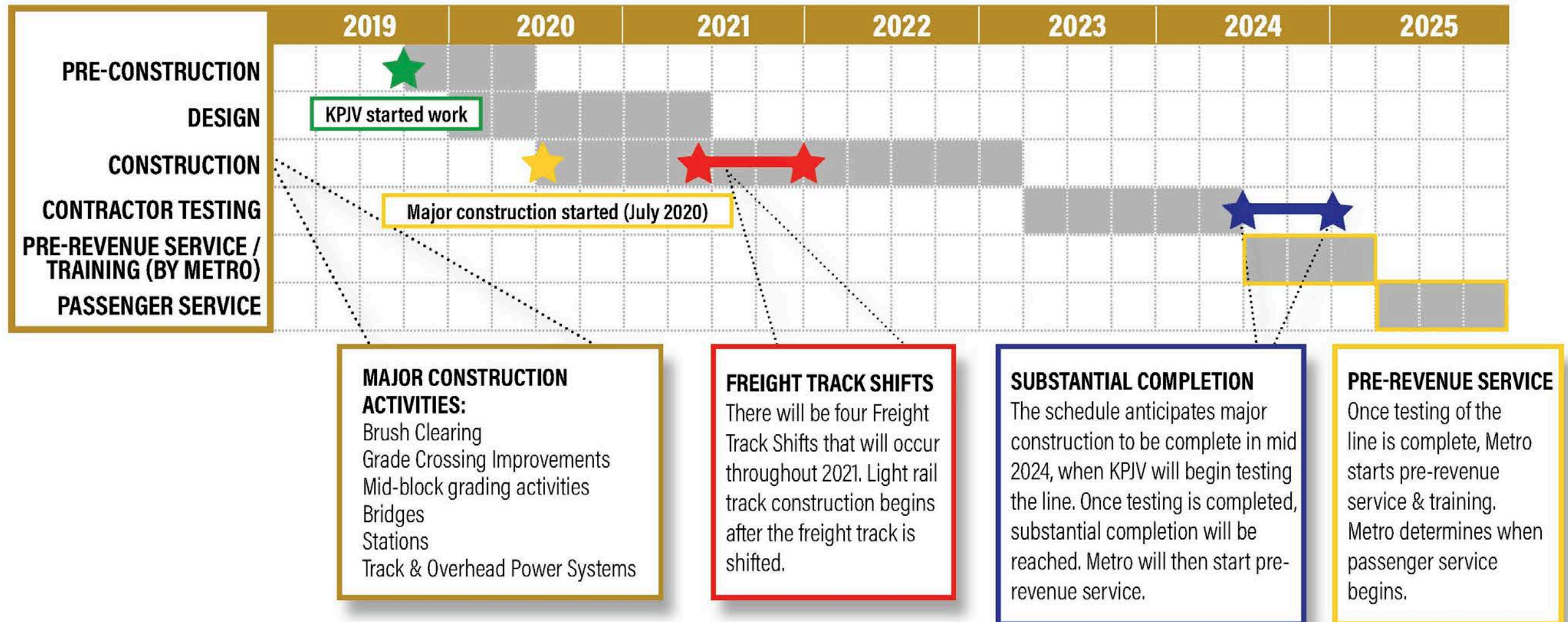
- The Construction Authority Board approved the design-builder's (KPJV) Baseline Schedule in June 2020, with a contractual deadline of substantial completion by January 3, 2025.
- The current project schedule forecasts a completion date three months early than contractually required.





# Summary Project Schedule (anticipated)

Foothill Gold Line Glendora to Pomona - Baseline Schedule



\*The Baseline Schedule submitted by KPJV to the Construction Authority in February 2020 is an aggressive schedule that aspires to reach Substantial Completion well ahead of the January 2025 contractual date. It is currently under review and is subject to change.

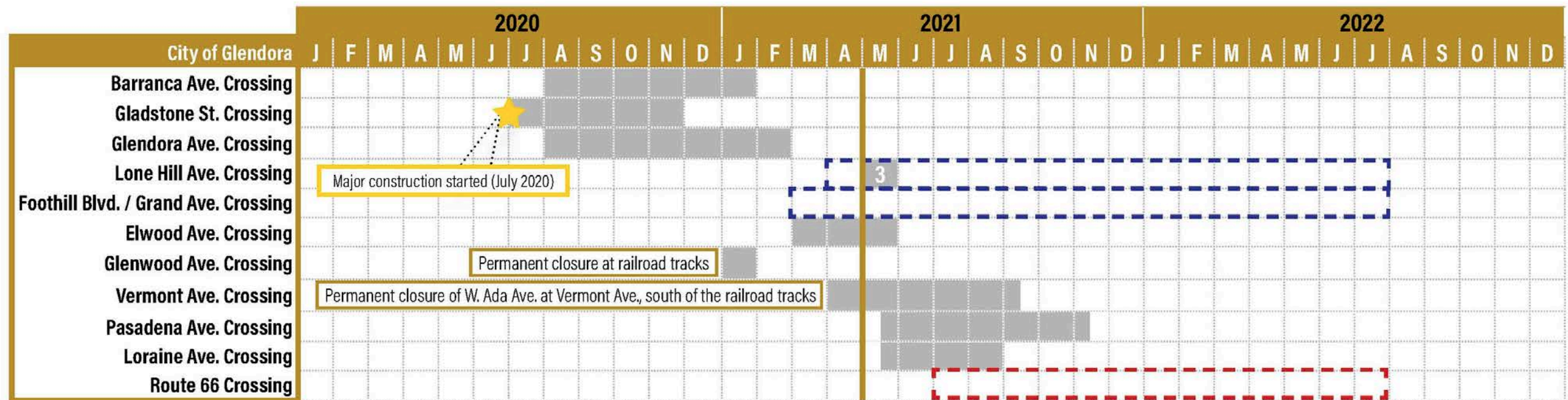
**Note:** If additional funding is secured by October 2021 to include the contract option to Montclair, the project will be completed altogether to Montclair in 2028 and this schedule will be adjusted.



**Foothill Gold Line**




# Anticipated Grade Crossing Schedule - Glendora



Grade crossing work includes constructing underground utilities, rebuilding the street, curb, gutter, and sidewalks; as well as installing new tracks and safety equipment for the freight and Gold Line Systems. Grade crossing construction will generally take between 3-6 months to complete at each location, some locations will require additional work after the Freight Track Shifts. As final designs are completed, these durations could increase or decrease due to specific grade crossing complexities.

**Note:** This timeline shows roadway construction and does not include all bridge construction at Foothill Blvd./Grand Ave., Lone Hill Ave., & Route 66. Minimal street impacts are anticipated throughout remaining bridge completion.

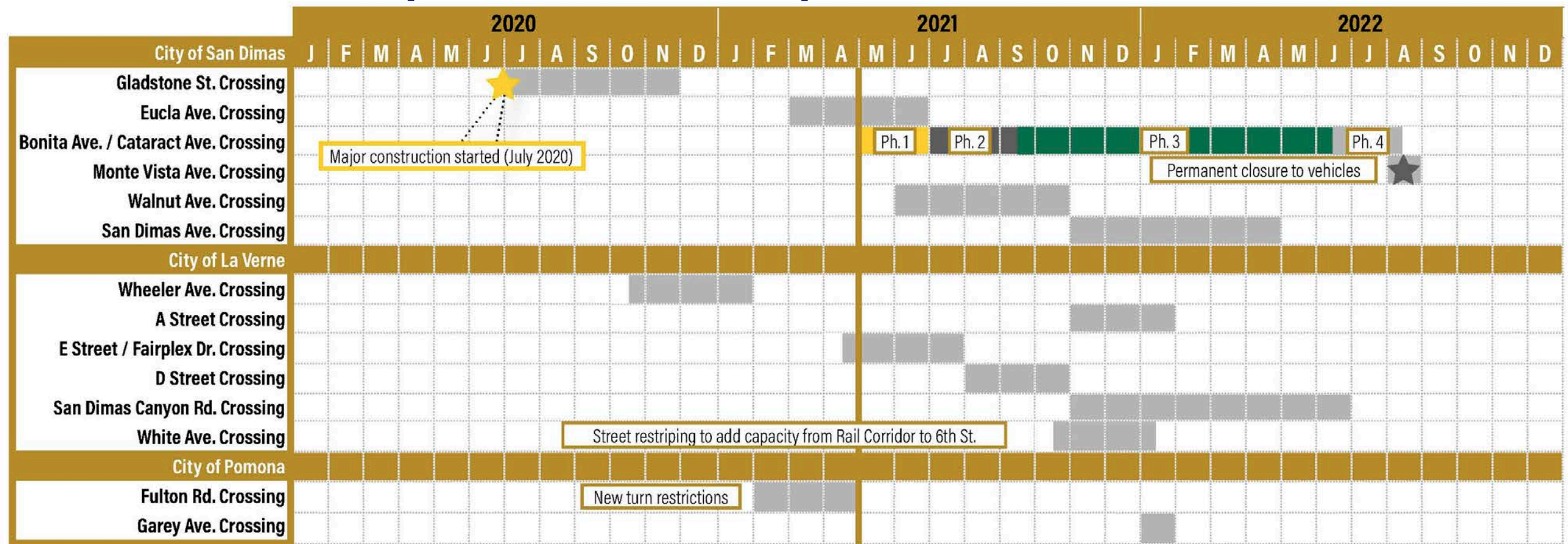
 Permitted nine days of full closure at these locations, 3 of which have been taken at Lone Hill Ave. (shown above). Intermittent lane closures will be taken within the duration shown in the schedule above.

 Intermittent lane closures will be taken within the duration shown in the schedule above.

\*Crossing schedule is based on the Baseline schedule and is subject to change.



# Anticipated Grade Crossing Schedule - San Dimas, La Verne, Pomona



Grade crossing work includes constructing underground utilities, rebuilding the street, curb, gutter, and sidewalks; as well as installing new tracks and safety equipment for the freight and Gold Line Systems. Grade crossing construction will generally take between 3-6 months to complete at each location, some locations will require additional work after the Freight Track Shifts. As final designs are completed, these durations could increase or decrease due to specific grade crossing complexities.

**Note:** This timeline shows roadway construction and does not include all bridge construction at Bonita Ave./Cataract Ave. Crossing construction for the Bonita Ave./Cataract Ave. intersection is currently broken down into 4 phases: **PHASE 1:** Current condition with thru traffic fully maintained. **PHASE 2:** Traffic realigned under traffic control, thru intersection traffic maintained. **PHASE 3:** Cataract Ave. fully closed with exception of EB Bonita/SB Cataract traffic maintained. **PHASE 4:** Bonita Ave. & Cataract Ave. full intersection closure, access to downtown businesses maintained. Minimal street impacts are anticipated throughout remaining bridge completion.

★ Monte Vista Ave. will permanently close to vehicular traffic. Final condition includes a pedestrian walkway for access to Bonita Ave. from the neighborhood on the south.

\*Crossing schedule is based on the Baseline schedule and is subject to change.



# Grade Crossings Update

Active	Start Date
Eucla Ave (San Dimas)	March 2021
E Street (La Verne)	April 2021
Vermont Ave (Glendora)	April 2021
Pasadena Ave (Glendora)	May 2021
Loraine Ave (Glendora)	May 2021
Upcoming	Estimated Start
Walnut (San Dimas)	June 2021
D Street (La Verne)	July 2021

Grade crossing improvements require extended full street closures.



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# Grade Crossings Update

Reconstructed Grade Crossing	Re-opened
Gladstone St (San Dimas)	November 2020
Barranca Ave (Glendora)	January 2021
Wheeler Ave (La Verne)	February 2021
Glendora Ave (Glendora)	March 2021
Fulton Ave (La Verne / Pomona)	April 2021
Elwood Ave (Glendora)	May 2021
Permanent Street Closures	Permanently Closed
Glenwood Ave (Glendora)	January 2021
West Ada Ave (Glendora)	April 2021





# Bridge Construction Sequencing



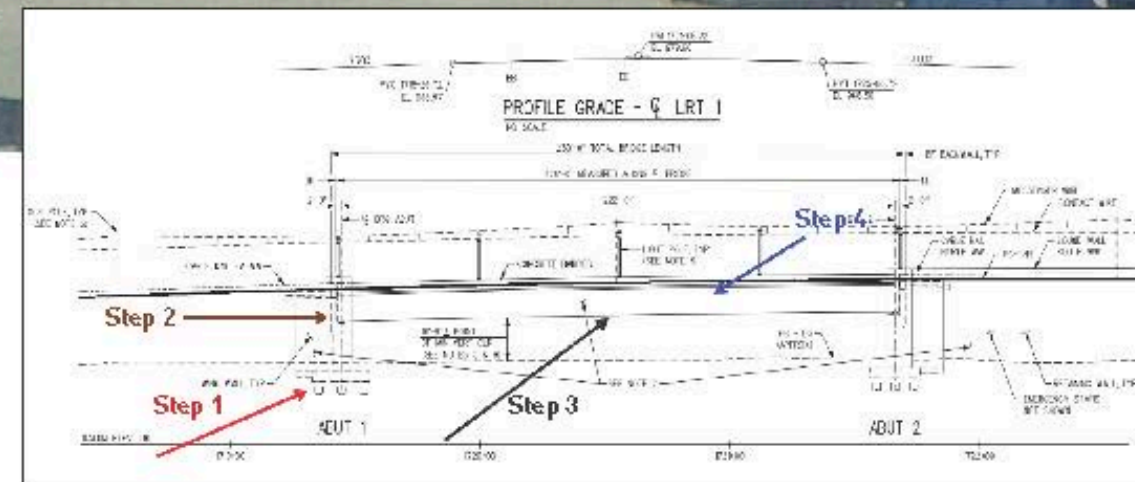
## Bridge Sequence:

### Sub structure (bridge support)

1. Excavate for foundations form work, place form work, install reinforcement, pour concrete bridge foundations
2. Form abutment walls & place rebar, pour abutment walls, strip abutment leaving abutment structure

### Super structure

3. Place bottom deck false work across entire bridge span; Falsework towers are generally placed at the mid-span of the super structure
4. Construct bridge bottom slab; Construct bridge outer and interior walls; Construct bridge top deck and barrier walls; Remove all falsework - leaving bridge super structure in its entirety



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# **Community Outreach**

**Lisa Levy Buch**

**Construction Authority**

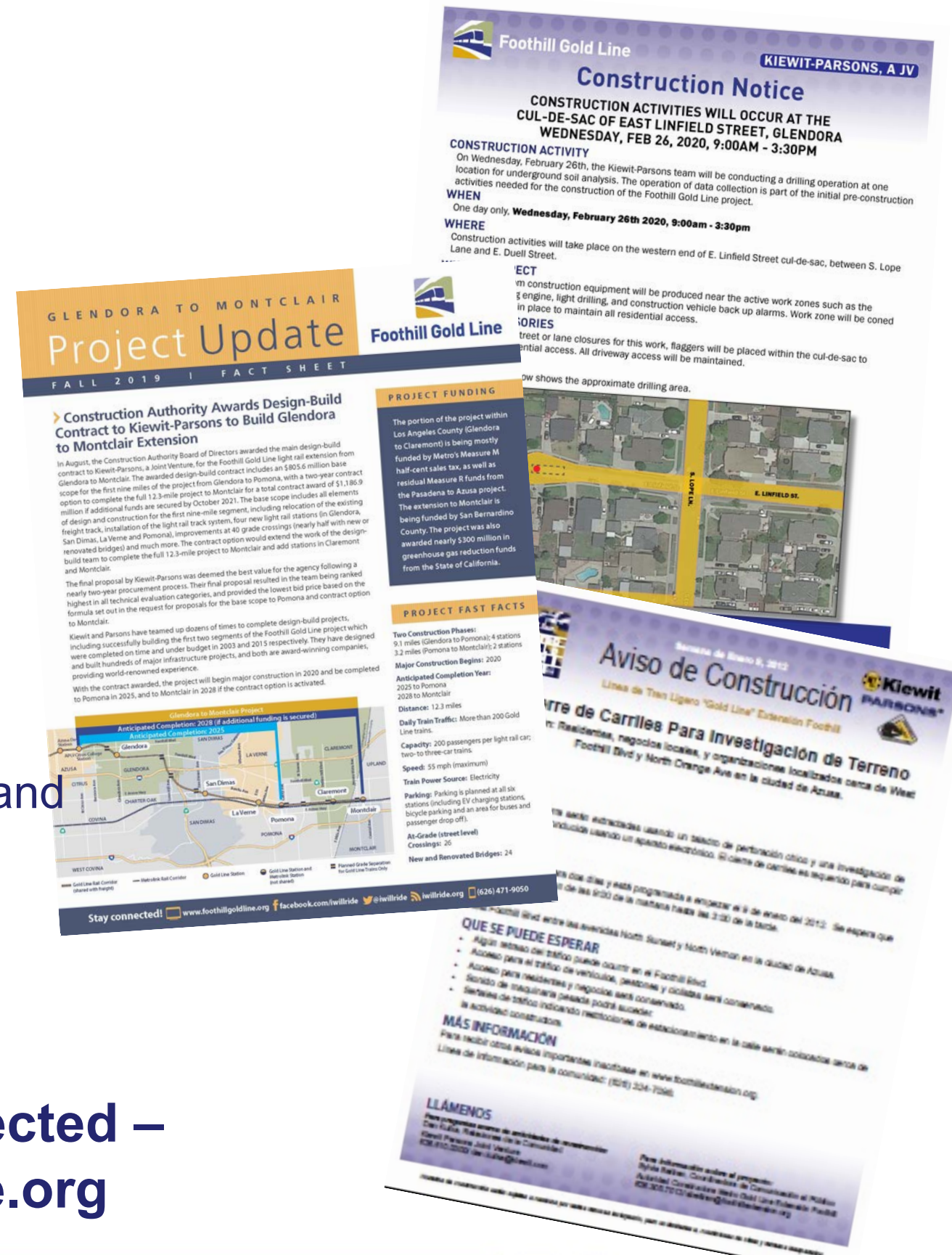
**Chief Communications Officer**



# Outreach Activities

- Construction Hotline/Email
- Construction Notices, distributed:
  - ✓ Interactive construction map – [foothillgoldline.org](http://foothillgoldline.org)
  - ✓ E-Alerts
  - ✓ Community walks
  - ✓ City websites
  - ✓ Authority website
- E-News Updates
- Twitter, Facebook and Blog Updates
- Community-Accessible Office in Glendora
- Construction Safety Program for Schools and Community (when allowed)
- Gold Line Tours (when allowed)

Sign Up to Stay Connected –  
[www.foothillgoldline.org](http://www.foothillgoldline.org)



Foothill Gold Line



# Stay Updated on the Foothill Gold Line

Learn the latest and sign up to receive E-News Updates and Construction Alerts:

[www.foothillgoldline.org](http://www.foothillgoldline.org)

Follow the project online & on social media:



/iwillride



@iwillride



iwillride.org

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## *Have Questions?*

### Project Construction Questions



(626) 513-5788



CommunityRelations@Kiewit-Parsons.com

### General Project Questions



(626) 471-9050



PublicAffairs@foothillgoldline.org



Foothill Gold Line





# **Pomona to Montclair Funding Update**



# Anticipated Project Schedule



## NOTES:

\*\* October 2021 - funding deadline to initiate contract option to Montclair, using firm, fixed price bid.

\*\*\* Construction Authority retains right to re-procure Pomona to Montclair segment if market conditions improve and funding becomes available.





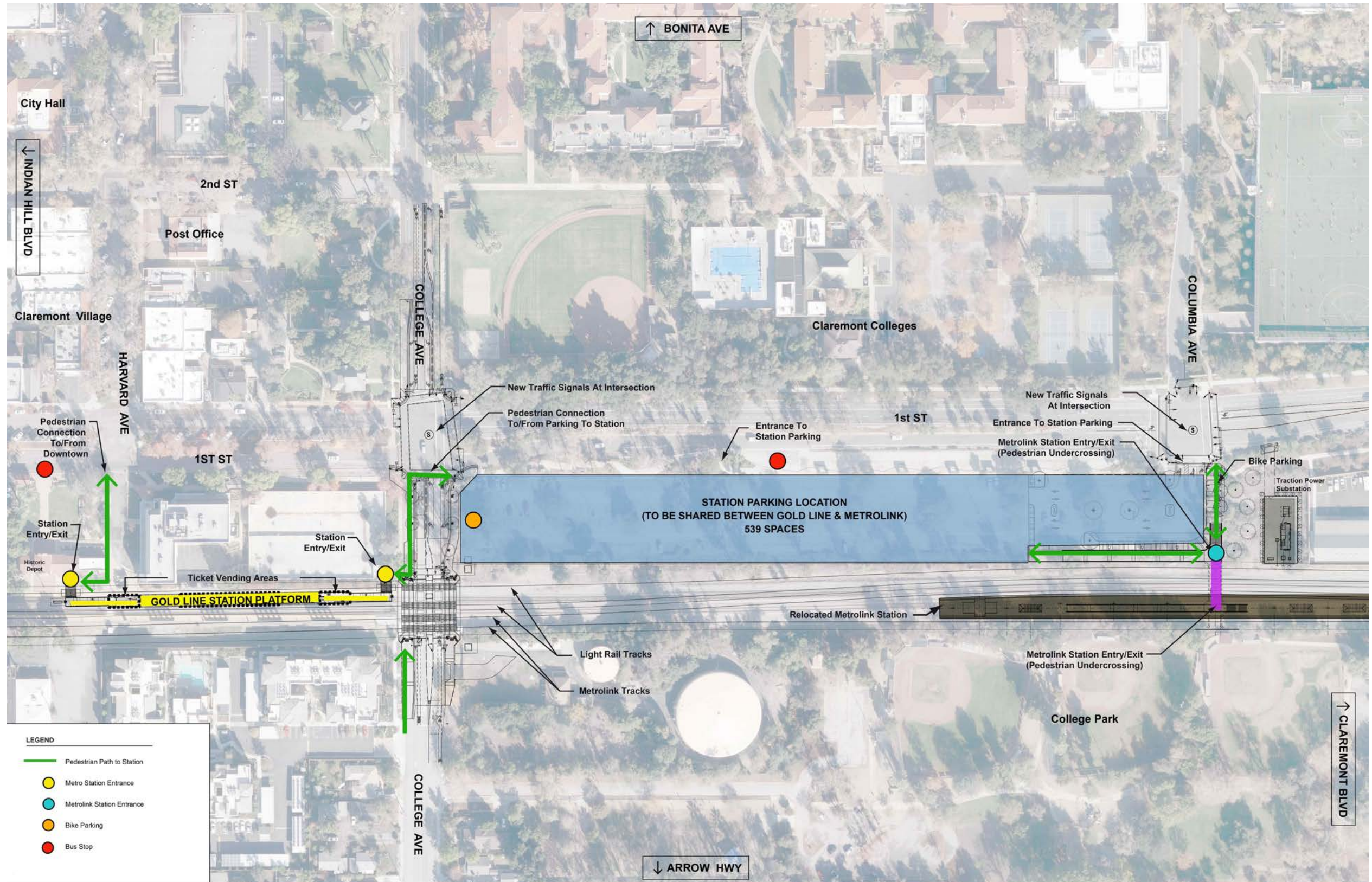
# Claremont Station Rendering



**Foothill Gold Line**



# Claremont Station Site Plan



As approved in January 2021, the parking facility will have approximately 539 spaces.



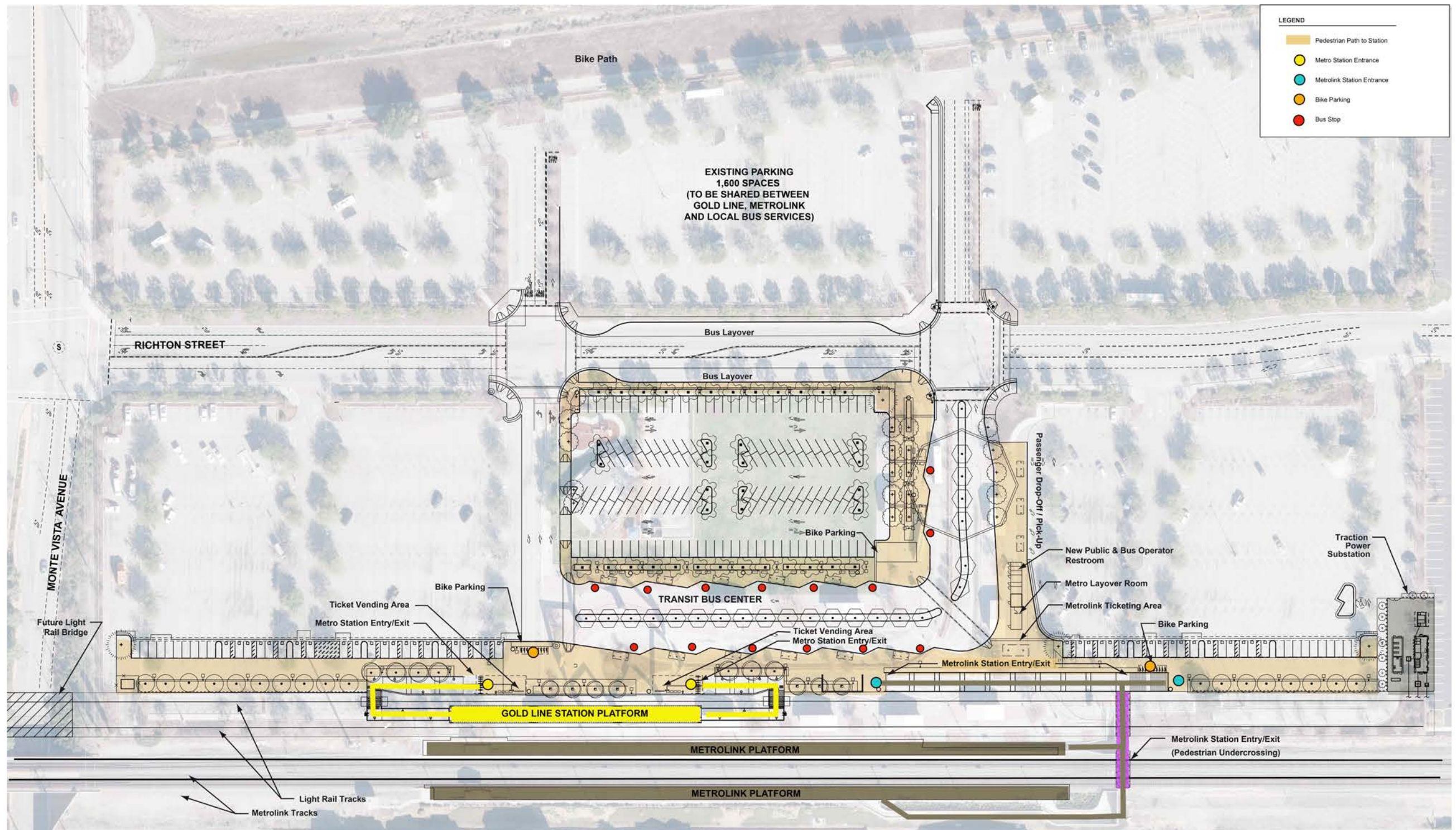
# Montclair Station Rendering



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# Montclair Station Site Plan







**foothillgoldline.org**  
**iwillride.org**



/iwillride



@iwillride